



AGENDA TITLE: Set Public Hearing for July 20, 2011 to Consider Resolution Approving

Harney Lane Specific Plan Report and Certifying Harney Lane Specific Plan Negative Declaration as Adequate Environmental Documentation for Harney Lane

Specific Plan

MEETING DATE: June 15,2011

PREPARED BY Public Works Director

RECOMMENDED ACTION: Set public hearing for July 20, 2011 to consider resolution approving

Harney Lane Specific Plan Report and certifying Harney Lane Specific Plan Negative Declaration as adequate environmental

documentation for Harney Lane Specific Plan.

BACKGROUND INFORMATION: The 2010 General Plan designates Harney Lane as a four-lane

expressway between Lower Sacramento Road and State Route 99. West of Lower Sacramento Road to the city limits, Harney Lane is generally designated to be a four-lane expressway. The Specific

Plan covers the area from the city limits on the west to South Stockton Street on the east, as presented in Exhibit A. The section of Harney Lane between South Stockton Street and State Route 99 is not included in the Specific Plan because the roadway widening is currently occurring as part of development in the area.

The Harney Lane Specific Plan is a planning tool that will be used to guide the systematic implementation of the adopted General Plan. It effectively establishes a link between implementing policies of the General Plan, individual development proposals, and future City capital improvement projects along the Harney Lane corridor. A copy of the Harney Lane Specific Plan Report and Technical Appendices are available at the Public Works Departmentfor review, and an abridged copy of the report is provided in Exhibit B.

The Harney Lane Specific Plan will accomplish two objectives. The first is, to identify the roadway improvements to be constructed along Harney Lane, the second, to establish the required right-of-way to serve anticipated growth and future traffic volume increases along the corridor.

The proposed geometrics reflect the ultimate improvements required to serve the traffic expected to result from development of the General Plan. It is anticipated that a phased-constructionapproach will be utilized to construct the ultimate improvements. Therefore, interim geometric plans will likely be implemented to deal with constraints represented by cost, relocation of structures, right-of-way acquisition, and accident history. As development occurs along the corridor, ultimate improvements will be required in conjunction with that development project.

An important aspect of the design of expressways is the limitation on the number of intersections and other turning movements onto and from the expressway. This Specific Plan assumes that intersections with median openings will be allowed only at Westgate Drive (a future intersection), Lower Sacramento Road, South Mills Avenue, South Ham Lane, South Hutchins Street, and South Stockton Street. Further discussion of the geometric design and phased improvement of Harney Lane is provided below.

APPROVED:

Konradt Bartlam, City Manager

Set Public Hearing for July 20, 2011 to Consider Resolution Approving Harney Lane Specific Plan Report and Certifying Harney Lane Specific Plan Negative Declaration as Adequate Environmental Documentation for Harney Lane Specific Plan June 15,2011
Page 2

The corridor is divided into four distinct segments for discussion purposes – Segment 1: City limits to Lower Sacramento Road; Segment 2: Lower Sacramento Road to Mills Avenue; Segment 3: South Mills Avenue to South Hutchins Street; and Segment 4: South Hutchins Street to South Stockton Street. This segmentation is not intended to reflect the phasing of improvements or the sequencing.

Segment 1 - City Limits to Lower Sacramento Road

Harney Lane west of Lower Sacramento Road lies primarily within San Joaquin County and partly within the City. The County portion of this road segment is anticipated to eventually be annexed into the City. The north side of Harney Lane in this area is within the City's sphere of influence. The south side is not within the City's sphere of influence and is expected to remain in the County.

The houses on the north side are set back a minimum of 27 feet from the existing right-of-way, while the houses on the south range from 2 to 25 feet back from the existing right-of-way. Because of this condition, two improvement alternatives were developed for this segment. The first is an interim alternative that is not expected to be needed until the Southwest Gateway project develops, which could be 10 to 15 years in the future. Since the houses on the south are closest to the road, the existing south right-of-way line was held, wherever possible, and the improvements were pushed to the north side. To construct the interim improvements, $9 \frac{1}{2}$ feet of right-of-way would be required on the north side of the street. On the south, the two properties closest to Lower Sacramento Road would be affected. The amount of right-of-way required at these locations varies from 0 to 3 feet. By taking this approach, no housing structures will be impacted.

The ultimate improvement alternative for Segment 1 would closely conform to the City standard section for a secondary arterial which measures 64 feet from curb to curb. The improvements would need to be installed when traffic from additional development determines that it is necessary. The houses on the north are still set back far enough that the extra right-of-way will not impact the houses directly but would reduce the size of the front yards by 10 feet. On the south side, 10 properties are affected. Three properties would need to be purchased to build the improvements.

Segment 2 - Lower Sacramento Road to South Mills Avenue

Similar to Segment 1, Harney Lane between Lower Sacramento Road and South Mills Avenue is expected to be widened in phases. Interim improvements will be implemented when the City deems traffic volumes or accident levels have created unsafe conditions. The south side of the roadway consists of rural residential homes built on fairly large lots. The homes are generally set well back from the existing roadway. The interim improvements will widen Harney Lane to allow the traffic to flow safely but reduce impact to residences on the south side. The ultimate improvements are not expected to be required until the properties on the south side of Harney Lane and easterly of this segment are developed.

Segment 3 - South Mills Avenue to South Hutchins Street

Because there are only three property owners on the south side of Harney Lane, and these properties are likely to develop in the future, the City potentially will only be involved in the construction of the interim alternative if traffic demands increase and safety becomes a concern. The ultimate alternative will likely be constructed in conjunction with development. The residential homes east of the Woodbridge Irrigation Canal on the north side have driveway access onto Harney Lane. This access will continue to be provided for and complemented with on-street parking and a bicycle lane. In this area, a total of 14 feet is provided on the north side of the roadway to accommodate the parking and bicycle lane.

Set Public Hearing for July 20, 2011 to Consider Resolution Approving Harney Lane Specific Plan Report and Certifying Harney Lane Specific Plan Negative Declaration as Adequate Environmental Documentation for Harney Lane Specific Plan June 15,2011 Page 3

Segment 4 - South Hutchins Street to South Stockton Street

This segment is unique to the others due to the Union Pacific Railroad crossing that exists in the middle of the segment, as well as special considerations required to support the existing agricultural/industrial uses at the southeast quadrant of the Harney Lane and South Hutchins Street intersection. The City has plans for a grade separation at the railroad crossing as presented in the Harney Lane/Union Pacific Railroad Grade Separation Feasibility Study (Mark Thomas and Company, 2010). The Grade Separation Feasibility Study will be presented to the Council at a later date to discuss alternative grade separation designs and the environmental impacts associated with each alternative.

The Harney Lane and South Hutchins Street intersection is expected to be constructed in two phases. Agricultural operations at the two large properties, Costa and Tsutsumi, on the south side of Harney Lane will be impacted by construction of the interim and ultimate improvements along this segment of the Harney Lane corridor. The Tsutsumi property operates a vineyard located between the Union Pacific Railroad and South Stockton Street. The Tsutsumi property will only be affected when the grade separation widens Harney Lane to Stockton Street. At that time the Tsutsumi property will have its driveways widened to accommodate westbound entry of harvesting equipment turning into the property.

The Costa property is located between South Hutchins Street and the Union Pacific Railroad. Each phase provides the necessary improvements to Harney Lane while limiting the impacts to the Costa agricultural operations. The first phase is an interim alternative. The second phase is the ultimate widening of Harney Lane along the Costa property.

The first phase widens the South Hutchins Street intersection to accommodate the proposed medical center at the southwest corner of the intersection. The first phase includes widening Harney Lane along a portion of the Costa property. The median along the Costa property would be striped to allow left turns into their driveway on Harney Lane. The improvements will include the creation of an eastbound pull-out lane to facilitate trucks turning into and out of a new driveway into the Costa's property. This work will require the removal of cherry trees within the existing right-of-way and within the orchard to allow for construction of the new driveway.

The second phase is the construction of a grade separation at the Union Pacific Railroad. The grade separation will be a City-sponsored project and will be constructed as soon as funding is available. With this phase, the Costa's will lose full access to both driveways on Harney Lane by virtue of the raised median that will be constructed with the grade separation.

City staff met with the Costas in three one-on-one meetings to discuss the impacts the Harney Lane Specific Plan would have on their operations and access to their property. The Costas expressed concerns about the effects the widening of Harney Lane would have on the access to and operations of their business. The Specific Plan addresses the concerns expressed by the Costas.

Public Outreach

Two public meetings were held at Henderson Community Day School. The first public meeting was held on June **8**, 2010, to present the preliminary Specific Plan, explain the purpose of a specific plan, request input on the plan and to give the attendees an opportunity to individually meet with City staff. A total of 52 people attended the meeting. A second public meeting was held on January 25, 2011, to present residents with an update addressing the comments from the first public meeting, the follow up individual meetings and staff modifications to the Specific Plan. A total of 53 people attended the second meeting.

As part of the public outreach, the City staff invited all residents within the project area to meet one-on-one with the project team to further explain the Specific Plan and to discuss the impact to their K:\WP\PROJECTS\STREETS\HarneyLaneSpecificPlan\CSetPHSpecPlan NegDec.doc 67/1/2011

Set Public Hearing for July 20, 201 Ito Consider Resolution Approving Harney Lane Specific Plan Report and Certifying Harney Lane Specific Plan Negative Declaration as Adequate Environmental Documentation for Harney Lane Specific Plan June 15,2011
Page 4

individual properties. Since not all of the citizens could attend the public meetings, *two* separate letters went out to all property owners whose properties would be impacted. A total of 55 residents took the opportunity to meet with the project team. The meetings were either held at the property owner's residence or at the Public Works office. The meetings were successful in providing information to the property owners and in resolving most of the property owners' concerns and issues.

Harney Lane Specific Plan Nenative Declaration

In accordance with the California Environmental Quality Act (CEQA) and the CEQA Guidelines, the City, as the lead agency, prepared an Initial Study and Mitigated Negative Declaration (Exhibit C) and published a Notice of Availability (NOA) announcing that the Harney Lane Specific Plan Draft Negative Declaration had been prepared and was available to the public for review. The NOA was submitted to the State Clearinghouse, distributed to local agencies, sent to interested persons, posted with the County Clerk's office, mailed to all property owners of record within a 300-foot radius of the project boundary, posted on the City's website and published in the *Lodi News Sentinel*. The 30-day window for review and comment on the draft Negative Declaration commenced on Tuesday, April 12,2011, and concluded on Wednesday, May 11, 2011. During the public review period, five comments were received on the proposed Negative Declaration [State Clearing House, California Valley Miwok Tribe, State Department of Transportation (Caltrans) District 10, San Joaquin County Council of Governments (SJCOG), and Union Pacific Railroad (UPPR)].

The letter from the State Clearinghouse notes that the Initial Study and proposed Mitigated Negative Declaration were circulated for a 30-day period review and that only Caltrans District 10 submitted a comment letter. It further notes that the review requirements for draft environmental documents have been fulfilled. The letter from California Valley Miwok Tribe notes change of their mailing address. Their comment is noted and City staff has updated their mailing address. The Caltrans letter indicates that a traffic impact study is required for this project in order to determine the proposed project's near-term and long-term impacts to State facilities. The City feels this issue has already been addressed via a previous Mitigated Negative Declaration (Harney Lane Interim improvements Project SCH#2010072040) and the General Plan EIR 2010 (SCH#2009022075). The proposed Harney Lane Specific Plan is an integral component of the City's General Plan 2010, which identifies Harney Lane as a four-lane expressway. No physical improvements or construction activities are proposed in conjunction with implementation of the Harney Lane Specific Plan. Subsequent development in the plan area, including all subdivisions, site plan reviews, planned development review, and conditional use permits will be subject to environmental review on a project-by-projectbasis. SJCOG notes the project limits are within a habitat zone and future developments would be subject to requirements in effect at the time the developments occur. Finally, the UPRR letter notes that a future grade separation at the Harney Lane railroad is needed. The City notes that planning for the grade separation has commenced and it anticipates completing the design and environmental review of the project at some time in the future.

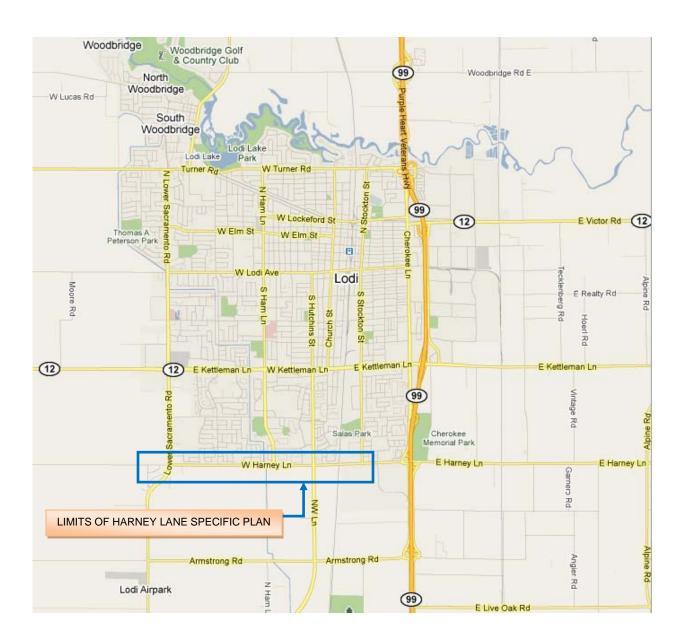
FISCAL IMPACT: Not applicable.

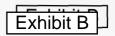
FUNDING AVAILABLE: Not applicable.

F. Wally Sandelin
Public Works Director

Prepared by Chris Boyer, Junior Engineer FWS/CB/pmf Attachrients

Exhibit A





HARNEY LANE SPECIFIC PLAN



Prepared for the City of Lodi Department of Public Works 221 West Pine Street Lodi, CA 95240

May 11,2011

Prepared by Mark Thomas & Company, Inc. 7300 Folsom Blvd., Ste 203
Sacramento, CA 95826



EXECUTIVE SUMMARY

The Specific Plan has two purposes. One purpose is to complete a public outreach program attempting to reach a consensus regarding the appropriate roadway improvements to be constructed along the Harney Lane corridor. The other purpose is to establish the required right of way to serve anticipated growth and future traffic voluine increases along the corridor. The Specific Plan was prepared in accordance with the City fi-amework set out in the General Plan adopted April 7, 2010. Items considered during the preparation of the Specific Plan include future land use, roadway and bicycle network, right of way issues, safety (pedestrian and vehicular), ingress and egress for residents and businesses and environmental issues.

The General Plan designates Harney Lane as a four lane expressway between Lower Sacramento Road and State Route 99. West of Lower Sacramento Road to the city limits, Harney Lane is designated to be a four lane minor arterial. The Specific Plan covers the area from the city limits on the west to South Stockton Street on the east. The section of Harney Lane between South Stockton Street and State Route 99 is not included in the Specific Plan because the roadway widening is currently occurring as part of development in the area. Traffic signals are located at Westgate Drive, Lower Sacramento Road, South Mills Avenue, South Ham Lane, South Hutchins Street/West Lane and South Stockton Street.

A total of 47 properties are affected by the planned widening of Harney Lane. The first of two public meetings was held on June 8, 2010 at the Henderson Community Day School to present the preliminary Specific Plan, explain the purpose of a specific plan, request input on the plan and to give the attendees an opportunity to individually meet with the City staff. A total of 52 people attended the meeting. Following the public meeting, individual meetings were conducted with 55 residents living along Harney Lane. A second public meeting was held on January 25,2011 at the Henderson Cominunity Day School to present residents with an update covering the comments fi-om the first public meeting, the follow up individual meetings and staff modifications to the Specific Plan. A total of 53 people attended the meeting.

TABLE OF CONTENTS

| INTRODUCTION | 4 |
|--|----|
| PURPOSE OF THE SPECIFIC PLAN | 4 |
| BACKGROUND | 4 |
| GENERAL PLAN | 5 |
| Land Use | 6 |
| Roadway Network | 6 |
| Bicycle Network | 6 |
| HARNEY LANE SPECIFIC PLAN | 11 |
| EXISTING CONDITIONS | 11 |
| PROPOSED GEOMETRIC DESIGN | 11 |
| Segment 1 – City Limits to Lower Sacramento Road | 12 |
| Segment 2 - Lower Sacramento Road to South Mills Avenue | |
| Segment 3 – South Mills Avenue to South Hutchins Street | 17 |
| Segment 4 - South Hutchins Street to South Stockton Street | 18 |
| Costa Property | 19 |
| Tsutsumi Property | 21 |
| Intersections - Full and Limited Access | 21 |
| PUBLIC OUTREACH | 24 |
| PUBLIC MEETINGS | 24 |
| ONE ON ONE MEETINGS | 24 |
| | |
| APPENDIX | |
| Appendix A Proposed Geornetrics | |
| Appendix B Intersection Geornetrics | |
| Appendix C _Costa and Tsutsuini Exhibits | |
| | |

TECHNICAL APPENDIX

Appendix E

—Public Meeting Reports and One on One Meeting Records

LIST OF FIGURES

| Figure 1 _Vicinity Map 5 |
|---|
| Figure 2 _City of Lodi General Plan Land Use Diagram7 |
| Figure 3 _City of Lodi General Plan Development Phases |
| Figure 4 _City of Lodi General Plan Roadway Network9 |
| Figure 5 _City of Lodi General Plan Bicycle Network 10 |
| Figure 6 Segment 1 Interim Roadway Section with Street Parking |
| Figure 7 _Segment 1 _Ultimate Roadway Section with Bicycle Lanes 14 |
| Figure 8 Segment 2 Interim Roadway Section Lower Sacramento Road to Legacy Estates |
| Figure 9 Segment 2 Interim Roadway Section Legacy Estates to South Mills Avenue 16 |
| Figure 10 Segment 2 and 3 Ultimate Roadway Section with Bicycle Lanes 16 |
| Figure 11 _Segment 3 _Ultimate Roadway with Bicycle Lanes and Street Parking On North |
| Figure 12 _Alternative 1 _Overhead with Side Slopes 19 |
| Figure 13 _Alternative 4 _Underpass with Side Slopes, 19 |

INTRODUCTION

PURPOSE OF THE SPECIFIC PLAN

The Harney Lane Specific Plan is a planning tool that will be used to guide the systematic implementation of the adopted General Plan. It effectively establishes a link between implementing policies of the General Plan, individual development proposals, and future City capital improvement projects along the Harney Lane corridor.

The Specific Plan has two purposes. One purpose of this Specific Plan is to complete a public outreach program attempting to reach a consensus regarding the appropriate roadway improvements to be constructed along the Harney Lane corridor. The second purpose is to establish the required right of way to serve anticipated growth and future traffic volume increases along the coil-idor. The Specific Plan was prepared in accordance with the City framework set out in the General Plan adopted April 7, 2010. Items considered during the preparation of the Specific Plan include future land use, roadway and bicycle network, right of way issues, safety (pedestrian and vehicular), ingress and egress for residents and businesses and environmental issues.

BACKGROUND

The City of Lodi is a town located in the northern poi-tion of San Joaquin County, California with an estimated population of 63,000 residents. Incorporated in 1906, the City has grown from its origin as a stop along the Union Pacific Railroad to a mix of manufacturing, light industrial, commercial, residential and agricultural uses. It is bordered on the north by the Mokelumne River and Central California Traction Railroad on the east. Generally, Harney Lane defines the City limit on the south.

The Cities of Lodi and Stockton are expanding towards each other. Lodi's sphere of influence extends one-half mile south of Harney Lane. As part of the City's General Plan an agricultural cluster study area was created to preserve a rural buffer between the two cities. Urban development will end approximately one half mile south of Harney Lane. A vicinity map of the area is provided in Figure 1.

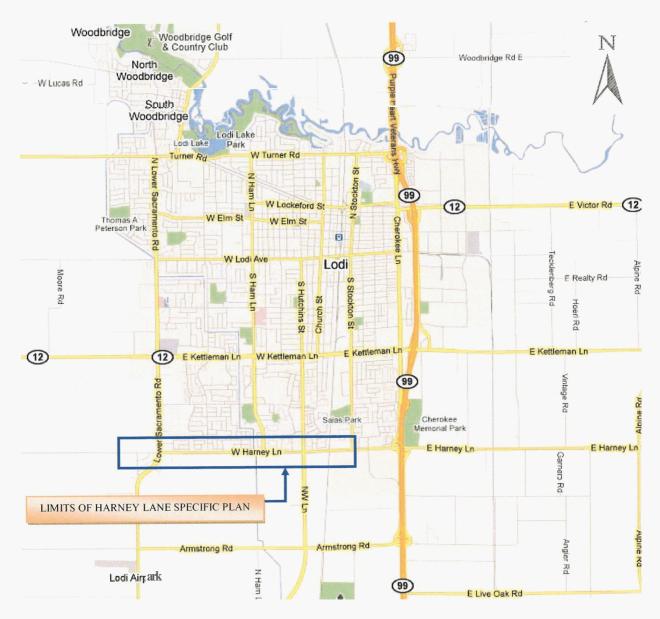


Figure 1 – Vicinity Map

GENERAL PLAN

The General Plan is the City's vision of how it will look 20 to 30 years in the future. The City of Lodi's General Plan was adopted by the City Council on April 7,2010. The General Plan established the mix of land uses along the Harney Lane corridor that will, in large part, be served by the circulation improvements to Hamey Lane and the adjoining properties.

Land Use

The City directs future growth and land uses based upon the General Plan. Along Harney Lane, the designated land uses are a mix of low, medium and high density residential, commercial, mixed use, schools and public parks. The land uses along the Harney Lane corridor designated by the General Plan are presented in Figure 2. Section 2.3 of the General Plan also included a discussion of the three phased implementation of the General Plan. The three phases are indicated on the map in Figure 3. Phase 1 includes the development of vacant land within the current city limits and development of the land south of Harney Lane. There is no time fi-ame predicted for when this development would occur.

Roadway Network

As part of the General Plan the City reviews the forecasted traffic volumes based on the anticipated growth of the city. The number of lanes for each of the roads in the network is determined fi-om these forecasts. A minimum peak hour Level of Service (LOS) "E" is permitted throughout the city recognizing that some level of traffic congestion during the peak hour is acceptable and that infi-astructure design should be based on the conditions that predoniinate during most of each day. A LOS of "E" translates to a maximum delay at an intersection (signalized) of 55 to 80 seconds. Other items that influence the size and look of a roadway are the posted speed and the access from side streets. Chapter 5 of the General Plan designates Harney Lane from Lower Sacramento Road to State Route 99 as a four lane expressway and west of Lower Sacramento Road it is designated as a minor arterial transitioning from the four lanes to two lanes to the west.

An expressway is a high speed-high volume road that has a raised median in the middle to separate the opposing traffic (for safety reasons) and limited access to help maintain the speed along the segment. A minor arterial connects a residential area to major arterials and/or expressways. West of the city the area is expected to remain agricultural and Harney Lane connects the agricultural area to the planned roadway network. Figure 4 shows the roadway network presented in the General Plan.

The General Plan establishes the future design of streets in new developments should generally match and extend the grid pattern of existing city streets. This is intended to disperse traffic and provide multiple connections to arterial or expressway streets. An intended benefit of the grid pattern is that citizens will be able to move within a developed area without having to access an arterial or expressway roadway.

Bicycle Network

Bicycle paths are classified as Class 1,2 and 3. A Class 1 bicycle path is an independent path only used by bicycles and pedestrians. A Class 2 bicycle path is part of the roadway with vehicular traffic using the traveled way while the bicycle path is separately marked on the shoulder of the road. A Class 3 bicycle path is a road where vehicles and bicycles share the traveled way. Harney Lane fi-om the City limit on the west to State Route 99 is designated as a Class 2 bicycle lane. Figure 5 presents the bicycle network from the General Plan.

FIGURE 2-1: LAND USE DIAGRAM

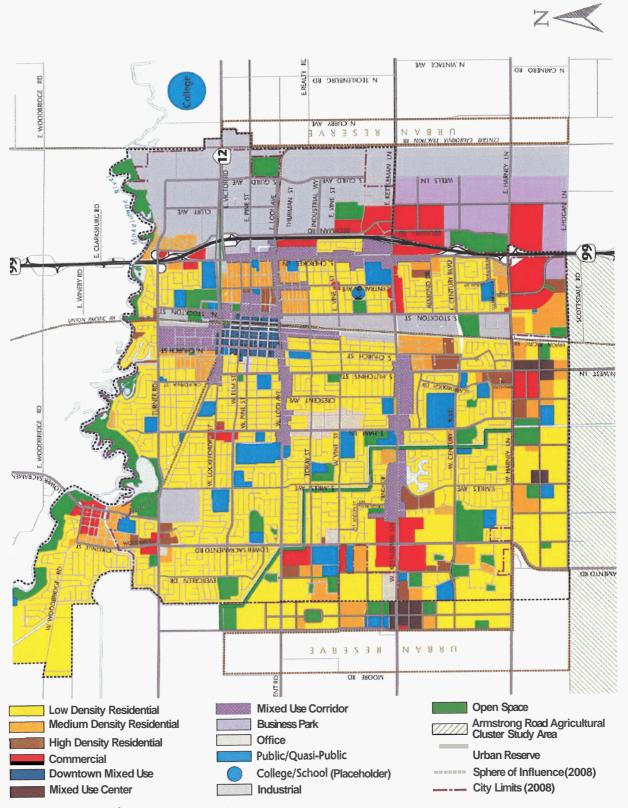


Figure 2 – City of Lodi General Plan Land Use Diagram

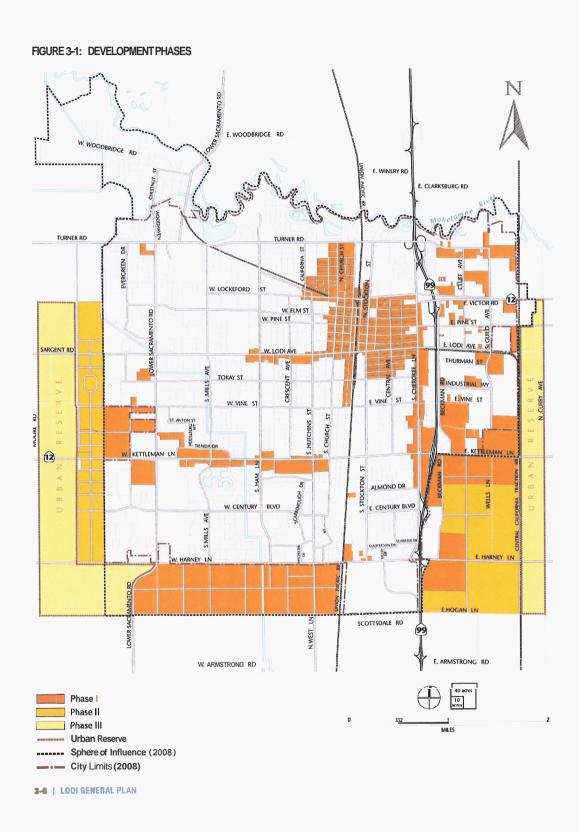
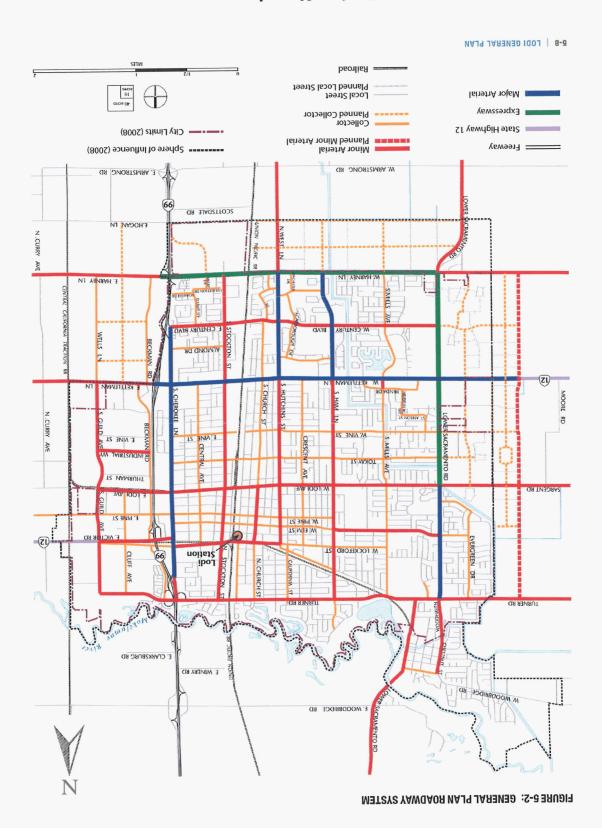


Figure 3 - City & Lodi General Plan Development Phases

Figure 4 - City of Lodi General Plan Roadway Network



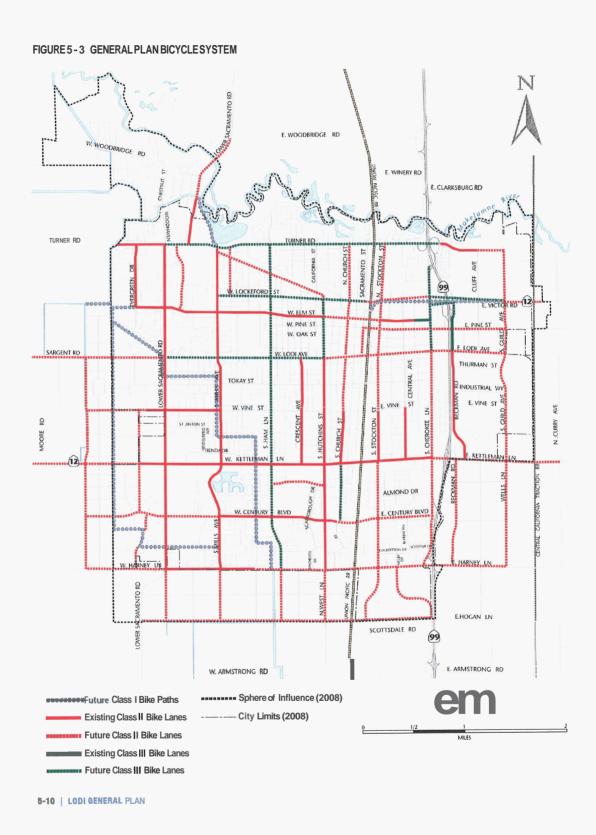


Figure 5 – City of Lodi General Plan Bicycle Network

HARNEY LANE SPECIFIC PLAN

EXISTING CONDITIONS

Hai-ney Lane is located at the southern edge of the current City limits and the City's development. The City limits run along the south side of the street right of way with San Joaquin County to the south. The City's sphere of influence extends further south to a point halfway between Harney Lane and Arinstrong Road.

Harney Lane is a two lane road with a posted speed of 45 mph. The study area includes approximately 2 ½ miles of roadway from the City's western limit to South Stockton Street that is partly within the City and partly within San Joaquin County. Ultimately, most of the roadway is anticipated to be annexed as future development occurs along the corridor.

Development along Harney Lane is a mix of urban residential, rural residential and agriculture. The north side of Hai-ney Lane is primarily single family residential development consisting of new subdivisions with block walls facing Harney Lane. Within the City limits, Harney Lane has been widened to the ultimate width on the north where recent development has occurred. The south side is a mix of agriculture (grapes and cherries) and rural residences. West and east of Lower Sacramento Road single family residences are present on both sides of the road and most are in San Joaquin County. Between South Hutchins Street and South Stockton Street, Harney Lane crosses the main Union Pacific Railroad.

PROPOSED GEOMETRIC DESIGN

Harney Lane is planned to be the primary east-west connector across the portion of the city south of Kettleman Lane. A second east-west connector is Century Boulevard as identified in the General Plan but completion of the crossing over the Union Pacific Railroad will not occur in the near future.

According to the General Plan Harney Lane is classified as a minor arterial west of Lower Sacramento Road and an expressway from Lower Sacramento Road to State Route 99. Lower Sacramento Road is also designated an expressway (the main north-south connector on the west side of the city) connecting Harney Lane to Kettleman Lane. The proposed geoinetrics for Hai-ney Lane reflect these secondary arterial and expressway designations and are provided in Appendix A.

The proposed geometrics reflect the ultimate improvements needed to serve the traffic expected to result from development of the General Plan. It is anticipated that a phased construction approach will be utilized to construct the ultimate improvements. Therefore,

interim geometric plans will likely be implemented to deal with constraints represented by cost, relocation of structures, right of way acquisition, and accident history.

In the event a specific development project occurs along the corridor, ultimate improvements will be required to be constructed in conjunction with that project. An example of this is the widening of Harney Lane that is presently under construction at the Reynolds Ranch Shopping Center Project.

Existing traffic signals at Lower Sacramento Road, South Hutchins Street and South Stockton Street will be modified in conjunction with the construction of interim and ultimate improvements at these intersections. In the case of the Lower Sacramento Road intersection, it is likely that two separate intersection improvement and traffic signal modification projects will be required due to right of way constraints.

In the event accident levels increase, it may be necessary to widen portions of Harney Lane to improve safe operations. An example of this would be a new traffic signal at an intersection with lane additions for turn movements and stacking. New traffic signals are planned for the intersections at South Mills Avenue and South Ham Lane. Another example may be widening the segment of the corridor currently within San Joaquin County east of Lower Sacramento Road.

An important aspect of the design of expressways is the limitation on the number of intersections and other turning movement onto and from the expressway. This specific plan assumes that intersections with median openings will be allowed only at Westgate Drive, Lower Sacramento Road, South Mills Avenue, South Ham Lane, South Hutchins Street, and South Stockton Street. Residential intersections without a median opening (right turn in and right turn out only) will be allowed only at Legacy Way, Poppy Drive, Winchester Drive and Banyan Drive on the north side of Harney Lane.

Further discussion of the geometric design and phased improvement of Harney Lane is provided below. The coi-ridor is divided into four distinct segments for discussion purposes. They are: Segment 1) City Limits to Lower Sacramento Road, Segment 2) Lower Sacramento Road to South Mills Avenue, Segment 3) South Mills Avenue to South Hutchins Street and Segment 4) South Hutchins Street to South Stockton Street. This segmentation is not intended to reflect the phasing of improvements nor the sequencing.

Segment 1 - City Limits to Lower Sacramento Road

Harney Lane west of Lower Sacramento lies primarily within San Joaquin County and partly within the City. The County portion of this segment is anticipated to be annexed into the City but probably not in the near future. The north side of Harney Lane in this area is within the City's sphere of influence but the south side is not and is expected to remain in the County. The area consists of single family residences on both sides of the road along with Henderson Community Day School on the south side. The houses on the north side are set back a minimum

27 feet from the existing right of way while the houses on the south are set back from 2 feet to 25 feet from the existing right of way.

The area is at the southwestern edge of the city limits and future development. Because of this, two iinproveinent alternatives were developed for this segment. The first is an interim alternative with a smaller right of way width of 69.5 feet designated to handle a smaller volume of traffic while minimizing the impacts to the residences on the south side. The interim alternative consists of three lanes. The three lanes include one lane in each direction and a middle two-way left turn lane. The two-way left turn lane improves the safety to the residents turning into their driveways while allowing the traffic to flow. Vehicle parking is provided for on both sides but may need to be restricted at the Lower Sacramento Road intersection. Sidewalk is included on both sides of the street and signal inodifications are necessary at Lower Sacramento Road. The sidewalk on the south side would not extend westerly beyond Henderson Community Day School.

Since the houses on the south are closest to the road, the existing south side right of way was held, wherever possible, and the improvements were pushed to the noi-th side. Nine and one half feet of right of way would be needed on the noi-th side of the street. On the south, the two properties closest to Lower Sacramento Road would be affected. The amount of right of way required at these locations varies from zero to three feet. By taking this approach, no housing structures were impacted. The geometric cross section of this alternative is presented in Figure 6 and details of the plan layout and cross sections are provided in Appendix A.

The interim alternative is not expected to be needed until development of the Southwest Gateway project occurs at its southern most limits and new street, Westgate Drive, west of Extension Road, has been constructed. The estimated year of construction for Westgate Drive is 2020. The year of construction of the interim alternative will be determined in the future based upon traffic volumes and operations but likely will not occur prior to 2025.

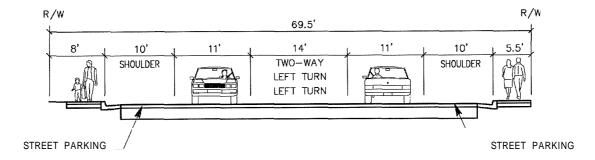


Figure 6 - Segment 1 - Interim Roadway Section with Street Parking (Looking East)

Segment 1 is designated as a minor arterial in the General Plan. The minor arterial designation is described as connecting the regional road network (Lower Sacramento Road and Harney Lane east of Lower Sacramento Road) with local roads with one typical characteristic being access limitations. The intent of the arterial designation was primarily to allow for greater access controls as future development occurs. Arterials, in the context of the General Plan may be either two or four lanes.

The ultimate improvement alternative for Segment I would closely conform to the City standard section for a secondary arterial which measures 64 feet from curb to curb including four travel lanes (48 feet) and two bicycle lanes (16 feet). The ultimate right of way would be 77.5 feet including an eight foot sidewalk and setback on the north side and a five and one-half foot sidewalk on the south side. The two and one-half foot setback on the south side is not provided to limit encroachment of the existing residences on the south side of Harney Lane. The geometric cross section diagram of the secondary arterial is provided in Figure 7 and details of the plan layout and cross sections are provided in Appendix A.

The iinproveinents would need to be installed when traffic from additional development determines that it is necessary. The additional right of way will be obtained from both sides of the street to align it with the Harney Lane iinproveinents east of Lower Sacramento Road. The houses on the north are still set back far enough that the extra right of way will not impact the houses directly but would reduce the size of the front yards by ten feet. On the south, ten properties are affected and three properties would need to be purchased to build the improvements. The three properties belong to the Geist (APN# 058-070-03), Hernandez (APN# 058-070-04), and Williains (APN# 058-070-05) families and are located nearest to Lower Sacramento Road.

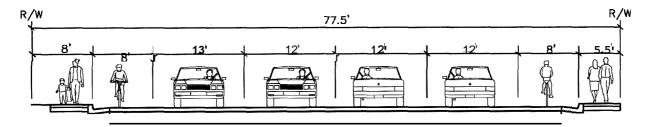


Figure 7 - Segment 1 - Ultimate Roadway Section with Bicycle Lanes (Looking East)

Segment 2 - Lower Sacramento Road to South Mills Avenue

Along this segment, Harney Lane is populated by single family residences along the south side and approximately one-half of the north side. The remainder of this segment on the north side has been developed as the Legacy Estates subdivision. The houses in the Legacy Estates subdivision do not fi-ont Harney Lane like the others do.

Similar to Segment 1, Harney Lane between Lower Sacramento Road and South Mills Avenue is expected to be widened in phases. They will again be referred to as interim and ultimate alternatives. Interim improvements will be implemented when the City deems traffic voluines or accident levels have created unsafe conditions. The cross section dimension for the interim alternatives varies by location along Segment 2. To simplify the discussion, two subsegments are presented below.

Lower Sacrainento Road to Legacy Estates (City Limit)

The existing residences along this sub-segment are located within San Joaquin County and development is not expected to take place nor are the properties anticipated to annex to the City. The interim alternative right of way requirement ranges from 62 feet to 69 feet. The interim improvements will be installed in portions of the segment that are not anticipated to experience development in the near future or ever at all. To reduce impacts to residences resulting from construction of the interim improvements, the widening will be limited to a four foot shoulder with no bike lanes or sidewalks. Between Lower Sacramento Road and Legacy Estates (City Limit) traffic will be separated by a striped median. It is anticipated the need for these interim improvements will be driven by the deterioration to unacceptable levels of the operations at the signalized intersection of Harney Lane and Lower Sacramento Road. These interim improvements add a westbound right turn lane at this intersection. The interim and ultimate alternative geometric designs for this sub-segment are presented in Figure 8 and 10 and Appendix A.

To construct the interim alternative, right of way acquisition is required on both sides of Harney Lane. On the north side, up to seven feet is required at the Schumacher (APN #058-230-10) and Galindo (APN # 058-230-06) properties. On the south side, seven feet is required along the Hayn (APN 058-070-07), Bell (APN 058-070-08), and Goff (APN 058-070-09) properties. At the time of acquisition, the City will need to work with San Joaquin County to determine if the acquisition will include only that needed for the interim alternative or include that needed for the ultimate alternative.

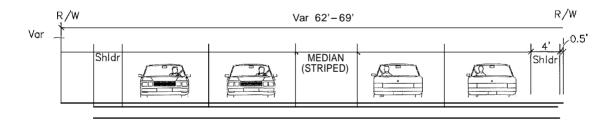


Figure 8 – Segment 2 – Interim Roadway Section – Lower Sacramento Road to Legacy Estates (Looking East)

Legacy Estates (City Limit) to South Mills Avenue

For the most part, the north side of Harney Lane along this sub-segment has been developed as the Legacy Estates subdivision. The south side of the roadway consists of rural residential homes built on fairly large lots. The homes are generally well set back from the existing roadway. Again, interim and ultimate alternative geometric designs for this sub-segment are presented in Figures 9 and 10 and Appendix A.

The interim alternative includes four travel lanes, center striped turning lane, full shoulder improvements (bike lane, sidewalk and landscaping) on the north side and a four foot shoulder

on the south side. Additional right of way acquisition is required on the south side that ranges from seven to twelve and one-half feet. The affected properties include Wright (APN 058-070-12), Velente (APN 058-070-13), S. Everitt (APN 058-070-14), Scholl (APN 058-070-15/16), Tamura (APN 058-090-01), Tanabe (APN 058-090-02), R. Everitt (APN 058-090-03), and Manassero (APN 058-090-04). On the north side, 17 feet of right of way will be required from Lackyard (APN 058-230-21), the property at the northwest comer of the Harney Lane and South Mills Avenue intersection to construct the interim alternative.

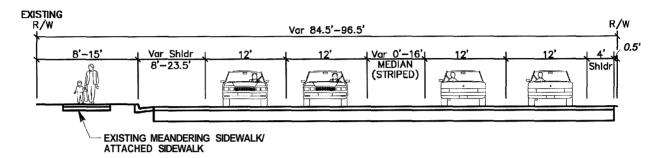


Figure 9 – Segment 2 – Interim Roadway Section – Legacy Estates to South Mills Avenue (Lookiizg East)

For the ultimate alternative the right of way dimension is 110 feet and includes four travel lanes, landscaped medianileft turn lane, bike lanes in each direction and fifteen feet behind the curb for five foot sidewalk, landscaping and utilities. Additional right of way acquisition is required on the south side that ranges from 26 to 31 feet. These amounts are inclusive of the right of way required to construct the interim alternative. It is expected that most of the right of way will be acquired through dedication at the time development occurs.

The General Plan includes a future "Local Street" accessing the area south of Harney Lane (See Figure 4). It is located west of Legacy Way. The location of the road shown in the specific plan is approximate. The exact location will be determined by the first property to develop that will be required to dedicate and construct the entire roadway. The road will have a 50 foot right of way, a 34 foot curb to curb dimension and include sidewalks on both sides.

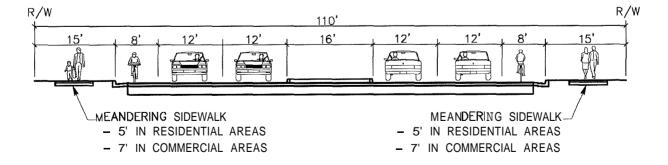


Figure 10 – Segment 2 and 3 – Ultimate Roadway Sectioiz with Bicycle Lanes (Looking East)

Segment 3 – South Mills Avenue to South Hutchins Street

Similar to the previous segment, existing single family residential development on the noi-th side sets the right of way limit of the ultimate improvements. Existing land use on the south side is agriculture. Within this area Harney Lane crosses a Woodbridge Irrigation District (WID) canal. The WID canal crossing was constructed several years ago and its design anticipated the widening of Harney Lane such that no changes to the crossing are required.

There is no interim alternative for Harney Lane within this segment. The Harney Lane ultimate cross section for this segment is the same 110 foot section as Segment 2 as presented in Figure 10 except at the WID canal crossing. At this location the meandering sidewalk is replaced by a roadway-contiguous sidewalk. The four lanes of traveled way and the median are maintained.

East of the canal to South Hutchins Street on the noi-th side of Harney Lane are nineteen existing residential propei-ties that have driveway access onto Harney Lane that will continue to be provided for and coinpliinented with on street parking and a bicycle lane. In this area a total of fourteen feet is provided on the noi-th side of the roadway to accommodate the parking and bicycle lane. The cross section for this poi-tion of Segment 3 is provided in Figure 11 and also in Appendix A.

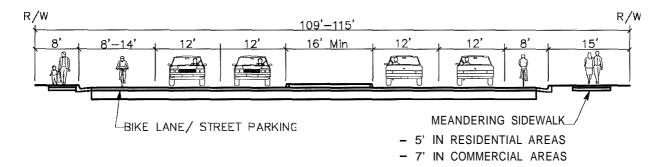


Figure 11 – Segment 3 - Ultimate Roadway with Bicycle Lanes and Street Parking On North (Looking East)

The houses on the north will be impacted by the construction of the ultimate iniprovements because the landscaped median will restrict the property owners to only turning west from their driveways. In addition, as traffic volumes increase on the roadway, the ability to freely back out of their driveway will be difficult during peak volume periods.

The General Plan includes a future "Local Street" accessing the area south of Harney Lane (See Figure 4). It is located near Poppy Drive. The location of the road shown in the specific plan is approximate. The exact location will be determined by the first property to develop that will be required to dedicate and construct the entire roadway. The road will have a 50 foot right of way, a 34 foot curb to curb dimension and include sidewalks on both sides.

Construction of the ultimate alternative will require acquisition or dedication of fifteen feet of additional right of way in the vicinity of the intersection of Harney Lane and South Hutchins Street. This includes an extra twelve feet of right of way near South Hutchins Street for a future dedicated right turn lane for the eastbound Harney Lane traffic to southbound South Hutchins Street. Along the remainder of this segment, the right of way required to construct the ultimate alternative was dedicated as part of the South Lodi Sanitary Sewer Study and the Harney Lane Lift Station project.

Segment 4 - South Hutchins Street to South Stockton Street

This segment is unique to the others due to the Union Pacific Railroad crossing that occurs in the middle of the segment and special considerations required to support the existing agricultural/industrial uses on the south side of Harney Lane. Information, phasing, and alternatives for Segment 4, the grade separation, and the Costa (APN 058-110-47) and Tsutsumi (APN 058-130-24) agricultural operations are separately presented below. The future right of way requirements along segment 4 are indicated on the diagrams provided in Appendix A.

Union Pacific Railroad Grade Separation

The City has plans for a grade separation at the railroad crossing as presented in the Harney Lane/Union Pacific Railroad Grade Separation Feasibility Study (Mark Thomas and Company, 2010). The feasibility study, dated August 10,2010, analyzed the benefits and costs of different alternatives. On the north side, the existing residential subdivision has been set back from Harney Lane in anticipation of a grade separation ultimately being constructed. A 30 foot right of way has been secured by the City on the Costa property, however, the grade separation project will require more right of way from the Costa's property and the Tsutsumi Property.

Right of way requirements for the four grade separation alternatives are the greatest for the two alternatives which include earth embankments in lieu of retaining wall structures. The two embankment alternatives include an overcrossing and an undercrossing of the railroad. The schematic cross sections for each Alternative 1 and 4 from the feasibility study are presented in Figures 12 and 13, respectively. Alternative 1 is an overcrossing structure with side slopes and this alternative requires the greatest area of new right of way. Alternative 4 is an undercrossing with side slopes and requires the second greatest area of new right of way.

An environmental review of the alternative grade separation crossings will be conducted by the City in conjunction with the preliminary engineering design for the project. Once the environmental review and alternative selection has been completed, full design of the grade separation project will commence and final determination of right of way requirements will be made. A copy of the Feasibility Study is included in the Technical Appendix.

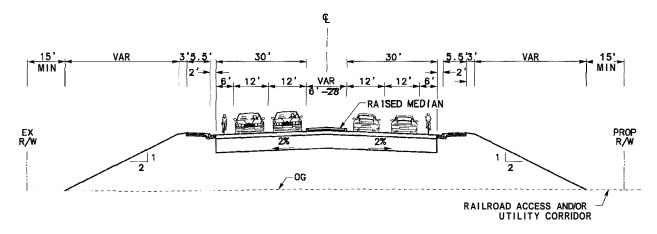


Figure 12 – Alternative 1 – Overhead with Side Slopes (Looking East)

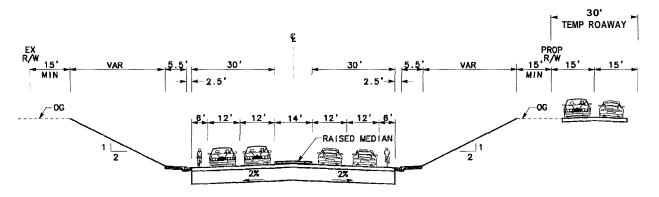


Figure 13 - Alternative 4 - Underpass with Side Slopes (Lookiizg East)

Agricultural operations at the two large properties, Costa and Tsutsumi, on the south side of Harney Lane will be impacted by construction of the interim and ultimate improvements along this segment of the Harney Lane corridor. The important issues associated with these property owners are described in the following two sections.

Costa Property

The Costa property is located between South Hutchins Street and the Union Pacific Railroad. The Costa's operate an agricultural packing plant in the center of their property which is accessed from West Lane and Harney Lane. The property has two driveways on Harney Lane and two driveways on West Lane used by employees, harvesting equipment and large trucks. The first driveway on Harney Lane is located just west of the railroad tracks and is used for access by large harvesting equipment. The second driveway is located just east of the South Hutchins Street intersection and is used for access by large trucks. The West Lane driveways will not be affected by the Harney Lane Specific Plan project.

The Harney Lane and South Hutchins Street intersection is envisioned to be constructed in two phases. Each phase provides the necessary improvements to Harney Lane while limiting the

impacts to the Costa property. The first phase is an interim alternative and the second phase is the ultimate widening of Harney Lane along the Costa property. The phasing exhibits are presented in Appendix C.

Phase 1: South Hutchins Street Intersection Improvements.

The first phase widens the South Hutchins Street intersection to accommodate the proposed medical center at the southwest corner of the intersection. The first phase includes widening Harney Lane along a portion of the Costa property to add a second through eastbound and westbound lane and relocation of the easterly driveway and gate to a point approximately 300 feet east of the South Hutchins Street intersection. The median along the Costa property would be striped to allow left turns into their driveway along Harney Lane. The improvements will include the creation of an eastbound pull out lane to facilitate trucks turning into the new driveway. This work will require the removal of cherry trees within the existing right of way and within the orchard to allow for construction of the new driveway. A diagram of the phase 1 improvements is provided in Appendix C.

▶ Phase 2: Construct the Railroad Grade Separation.

A grade separation at the Union Pacific Railroad is required to add two additional through lanes to Harney Lane. Unlike the other sections of Harney Lane, the grade separation will not be constructed in association with adjacent development. The grade separation will be a City sponsored project and will be constructed as soon as funding is available.

With this phase, the Costa's will lose full access to both driveways on Harney Lane by virtue of the raised median that will be constructed with the grade separation. Additional right of way will be needed. The amount will be determined once the preferred design alternative is selected as part of the environmental review of the project. The Costa driveway west of Banyan Drive will be restricted to right turn in and right turn out movements. The driveway would be treated like an intersecting street without a pull out lane the same as for the Banyan Drive intersection on the north side of Harney Lane and all other intersections along Harney Lane. The driveway will be approximately 300 feet east of the South Hutchins Street intersection in order to locate it away from the inclined portion of the grade separation.

When the property is developed into something other than the Costa's agricultural operation the driveway will be the approximate location for the future road. The roadway will be required to be dedicated at that time. The road will have a 50 foot right of way, a 34 foot curb to curb dimension and include sidewalks on both sides.

A diagram of the phase 2 improvements is provided in Appendix C. The right of way requirements based on Figures 12 and 13 are indicated in the phase 2 diagram and other exhibits in Appendix A and C.

City staff met with the Costas in three one on one meetings to discuss the impacts the Harney Lane Specific Plan would have on their operations and access to their property. The Costas expressed concerns about the effects the widening of Harney Lane would have on the access to and operations of their business. The specific plan addresses the concerned expressed by the Costas.

Tsutsumi Property

Mr. Tsutsumi operates a vineyard located between the Union Pacific Railroad and South Stockton Street. Access to the vineyard is available at two locations on Harney Lane, west of South Stockton Street. A row of houses is situated between the Tsutsumi property and South Stockton Street blocking access to South Stockton Street.

There is no interim alternative for Harney Lane within this area. The ultimate construction of the railroad grade separation will affect the operations at the Tsutsunii property. The grade separation will widen Harney Lane and construct a raised median. The raised median prevents the harvesting equipment from making westbound left turns into the Harney Lane driveway. The harvesting equipment will need to enter the property fi-om the eastbound approach. The driveways on Harney Lane will need to be widened beyond their current width to accoininodate the trucks turning in the property. A diagram of the Tsutsuini access configuration is provided in Appendix C.

Mr. Tsutsuini is amenable to this option as well as any other option which would maintain his operation. He suggested having the City obtain access to his property from the extension of South Stockton Street by buying one of the houses facing South Stockton Street.

Intersections - Full and Limited Access

The expressway design for Harney Lane between Lower Sacramento Road and State Route 99 will limit full access intersections by spacing them approximately one half mile apart. The planned full access intersections are Westgate Drive, Lower Sacramento Road, South Mills Avenue, South Ham Lane, South Hutcliins Street, South Stockton Street and Reynolds Ranch Parkway/Melby Avenue. The Reynolds Ranch Parkway/Melby Avenue intersection has been constructed as part of the Reynolds Ranch project.

Limited access intersections are those that restrict turning movements to right turns from and right turns onto Harney Lane with a prohibition of left turn movements. Planned limited access intersections include Legacy Way, Crown Place, Poppy Drive, Winchester Drive, Banyan Drive and the four future road intersections on the south side of Harney Lane. A short discussion about the improvements at each full access intersection is given below. A diagram of the geonietrics for each intersection is included in Appendix B.

- 1. **Westgate Drive** Westgate Drive, the future road across from the Henderson Community Day School, is part of the Southwest Gateway planned development. The road will form a three-legged intersection on Harney Lane from the noi-th and will be striped with crosswalks due to its proximity to the school. When the Southwest Gateway development is constructed the timing of the intersection construction will be determined. It is included in the interim and ultimate proposed geoinetrics for this segment of Harney Lane. A traffic signal will be installed in the future if required.
- 2. Lower Sacramento Road Lower Sacramento Road is the main north-south connector between Stockton and Lodi on the west side of Lodi. Between Kettleman Lane and Harney Lane, Lower Sacramento Road is designated an expressway. It is a main part of the city's grid network planned to serve large volumes of traffic moving from Kettleman Lane to the southeast area of Lodi. Harney Lane is part of this connection. To serve the high volume of southbound to eastbound left turn traffic, dual left turn lanes are incorporated into the intersection geometrics. The existing traffic signal at the intersection will need to be modified to accommodate the additional through lanes and left turn lane.
- 3. **South Mills Avenue** South Mills Avenue is a collector street that will be extended southward as new development occurs. It will become a full access intersection on Harney Lane. Signals will be installed with the extension of South Mills Avenue. A single left turn lane will be provided for eastbound and westbound traffic on Harney Lane. The intersection is currently signalized and will need to be modified to accommodate the widening.
- 4. **South Ham Lane** Similar to South Mills Avenue, South Ham Lane currently tees into Harney Lane and will be extended southward as new development occurs. The General Plan designates South Ham Lane as a major arterial to the noi-th of Harney Lane and a collector road to the south. Signals will be installed with the extension of South Ham Lane and single left turn lanes will be provided for eastbound and westbound Harney Lane traffic.
- 5. **South Hutchins Street** Similar to Lower Sacramento Road this is a main connector road between Lodi and Stockton. In the General Plan South Hutchins Street is a major arterial which serves the downtown area of the city. At the intersection it will have dual left turns for the north and southbound traffic. Single left turn lanes are provided for the Harney Lane traffic. Two eastbound and westbound through lane will be provided.

Traffic studies indicate the future requirement for a dedicated right turn lane for the eastbound to southbound turning movement on Harney Lane. In keeping with the General Plan policy to promote pedestrian friendly intersections this dedicated right turn lane is included in the specific plan but is not recommended for construction in the foreseeable future.

Right of way for the additional lanes is required on the south side of Harney Lane as indicated on the diagrams provided in Appendix A. The intersection is currently signalized and will need to be modified to accommodate the widening.

6. South Stockton Street – The roadway tees into Harney Lane with a dead end road extending *to* the south. In the future the dead end road will be widened allowing access to the Reynolds Ranch project in the south area. Similar to South Hutchins Street intersection the eastbound Harney Lane will initially be served by a through lane and a combined through/right turn lane. When South Stockton Street is extended and the traffic wail-ants it, the combined through/right turn lane will become a through lane and a dedicated right tui-n lane will be added. The right of way is reserved with the Specific Plan. The existing signal will need to be modified. The intersection is currently signalized and will be modified to accommodate the widening.

PUBLIC OUTREACH

PUBLIC MEETINGS

Two public meetings were held at Henderson Community Day School. The first public meeting was held on June 8,2010 to present the preliminary specific plan, explain the purpose of a specific plan, request input on the plan and to give the attendees an opportunity to individually meet with the City staff. A total of 52 people attended the meeting. A second public meeting was held on January 25,2011 to present residents with an update addressing the comments from the first public meeting, the follow up individual meetings and staff modifications to the specific plan. A total of 53 people attended the second meeting. A full report on each meeting is provided in the Technical Appendix.

The format of the two public meetings was an open house with work stations exhibiting the four segments. A staff member was present to explain the specific plan and to answer questions at each station. A power point presentation was given describing the need for the specific plan, the proposed geometrics, the timing of the implementation and contact information which the residents could use if they had further questions. Questions were received and answers provided as reported in the minutes of the meetings.

ONE ON ONE MEETINGS

As part of the public meeting, the City staff gave the residents an opportunity to meet later for a one on one meeting to explain the specific plan further and discuss the impact to their property. Since not all of the citizens could attend the public meeting two separate letters went out to all the property owners whose properties would be impacted. This outreach provided them additional opportunities to have a one on one meeting with the City staff to discuss the specific plan and their property. A total of 55 residents took this opportunity to meet with the project staff.

The meetings were either held at the property owner's residence or at the Public Works office. Most of the meetings were informational since many of these people were not able to attend the public meeting. Discussion points included:

- What is the purpose of the Specific Plan? The Specific Plan acts as a blue print of what Harney Lane will look like based on the policies set foi-th in the General Plan. City staff will use the specific plan to guide development of properties along Harney Lane to provide for implementation of the Harney Lane expressway.
- How much right of way will be needed from my property? An exhibit for each property was created showing the amount of right of way needed. If the property was

located in a segment where an interim condition occurred, a second exhibit was created showing the right of way needed during the interim phase.

- When is this development and street widening going to occur? This was a significant concern and a common question. It was explained that the specific plan was a planning document to be used by the City to direct the individual developers as to what would be expected of them with respect to the widening of Harney Lane. The street widening was not expected to occur in the near future. Much of the widening will occur with development. If widening was required, the City would acquire right of way through a formal acquisition process.
- How will I get compensated for the right of way taken? The City will require a development project to dedicate needed right way as part of the project. Acquisition of right of way outside the limits of new development may follow an informal process if initiated by a developer or a formal process if initiated by the City.
- By acquiring the right of way the house will be closer to the roadway making the property less valuable. Will I be compensated for that? This is referred to as "severance damages" and would be included in the offer to acquire the property.

There were some questions and concerns that were brought up from the residents that needed follow up on the staffs part. These concerns were:

- For the segment west of Lower Sacramento Road, by acquiring right of way, you are reducing the size of the driveway and front yards; why not include street parking for the residences? The recommend geometrics include street parking.
- Why does the median have to be so wide? Less property would need to be taken if you reduced the size of the median. The median is sixteen feet wide and provides room for the left turn pockets and narrow median at the intersections.

Draft Initial Study/Negative Declaration



HARNEY LANE SPECIFIC PLAN

April 2011

Draft

Initial Study/Negative Declaration

For

HARNEY LANE SPECIFIC PLAN

April 2011

Prepared by the City of Lodi Department of Public Works 221 West Pine Street Lodi, CA 95240

TABLE OF CONTENTS

| Sect | tion 1: Introduction | 1-1 |
|------|--|------|
| | 1.2 - Lead Agency | 1-1 |
| | 1.3 - Purpose and Document Organization | 1-2 |
| | 1.4 - Incorporation by Reference | 1-3 |
| Sect | tion 2: Project Description | 2-1 |
| | 2.1 - Project Background | 2-1 |
| | 2.2 - Project Location | 2-1 |
| | 2.3 - Project Purpose and Objectives | 2-1 |
| | 2.4 - Project Description | 2-1 |
| | 2.5 - Leady Agency Name and Address | 2-9 |
| | 2.6 - Contact Persons and Phone Numbers | 2-9 |
| | 2.7 - Project Sponsor's Name and Address | 2-9 |
| | 2.8 - General Plan Designations | 2-9 |
| | 2.9 - Zoning Designations | 2-9 |
| | 2.10 - Project Construciton | 2-9 |
| | 2.11 - Required Project Approvals | 2-10 |
| | 2.12 - Other Project Assumption | 2-10 |
| | 2.13 - Techincal Studies | 2-10 |
| Sect | tion 3: Environmental Determination | 3 |
| | 3.1 - Environmental Factors Potentially Affected | 3-1 |
| | 3.2 - Environemntal Determination | 3-1 |
| | 3.3 - Notice of Availability | 3-2 |
| | 3.4 - Proposed Mitigated Negative Declaration | 3-4 |
| Sect | tion 4: Discussion of Environmental Evaluation | 4 |
| | 4.1 Aesthetics | 4-1 |
| | 4.2 Agricultural Resources | 4-4 |
| | 4.3 Air Quality | 4-7 |
| | 4.4 Greenhosue Gas Emissions | 4-10 |
| | 4.5. Biological Resources | 4-12 |
| | 4-6 Cultural Resources | 4-15 |
| | 4-7 Geology and Soils | 4-17 |
| | 4.8 Hazards and Hazardous Materials | |

| 4.9 | Hydrology and Water Quality | 4-24 |
|---------------|------------------------------------|------|
| 4.10 | Land Use and Planning | 4-28 |
| 4.11 | Mineral Resources | 4-30 |
| 4.12 | Noise | 4-31 |
| 4.13 | Population and Housing | 4-34 |
| 4.14 | Public Services | 4-36 |
| 4.15 | Recreation | 4-38 |
| 4.16 | Transportation/Traffic | 4-39 |
| 4.17 | Utilities and Service Systems | 4-42 |
| 4.18 | Mandatory Findings of Significance | 4-46 |
| Section 5: D | Picuments Referenced | 5-1 |
| LIST OF EX | CHIBITS | |
| Exhibit 1: Lo | ocation Map | 2-3 |
| Exhibit 2: Ex | xisting Land Use Diagram | 2-4 |
| Exhibit 3: Do | evelopment Phases | 2-5 |
| Exhibit 4: A | erial Map | 2-6 |
| Exhibit 5: H | arney Lane Specific Plan Design | 2-8 |

This document is divided into the following sections:

1.0 INTRODUCTION

This section provides an introduction and describes the purpose and organization of this document.

2.0 PROJECT DESCRIPTION

This section provides a detailed description of the proposed project and any alternatives considered.

3.0 ENVIRONMENTAL DETERMINATION

This section provides a summary of environmental factors that would be would be potentially affected by this project as indicated by the checklist on the following pages.

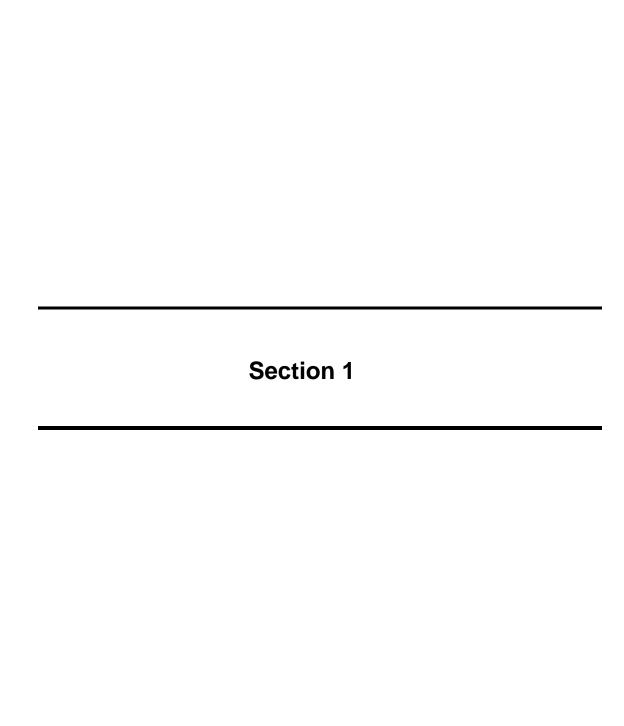
4.0 INITIAL STUDY CHECKLIST

This section describes the environmental setting for each of the environmental subject areas, evaluates a range of impacts classified as "no impact", "less than significant

impact", "less than significant with mitigation incorporated", or "potentially significant" in response to the environmental checklist, and provides mitigation measures, where appropriate, to mitigate potentially significant impacts to a less than significant level; and provides an environmental determination of the project.

5.0 DOCUMENTS REFERENCED

| ord · · · | • 1 | C '' .' | ((1 | 1 |
|--------------|--------------------|----------------------|-------------------|----------------|
| This section | provides a summary | z of mifigation mea | sures for the nro | nosed project |
| THIS SCCHOIL | provides a sammar | y of minigation mica | build for the pro | posca project. |



1.1 - INTRODUCTION AND REGULATORY GUIDANCE

The document is an Initial Study (IS) with supporting environmental studies, which provides justification for a Negative Declaration (ND) pursuant to the California Environmental Quality Act (CEQA) for the City of Lodi Harney Lane Specific Plan project (Project) in the City of Lodi. The IS/ND is a public document to be used by the City of Lodi (City) acting as lead agency, to determine whether the project may have a significant effect on the environment pursuant to CEQA.

If the lead agency finds substantial evidence that any aspect of the project, either individually or cumulatively, may have a significant effect on the environment that cannot be mitigated, regardless of whether the overall effect of the project is adverse or beneficial, the lead agency is required to prepare an Environmental Impact Report (EIR), use a previously prepared EIR and supplement that EIR, or prepare a subsequent EIR to analyze the project at hand.

If the agency finds no substantial evidence that the project or any of its aspects may cause a significant impact on the environment with mitigation, an MND shall be prepared with a written statement describing the reasons why the proposed project would not have a significant effect on the environment, and therefore, why it does not require the preparation of an EIR (State CEQA Guidelines Section 15371).

According to State CEQA Guidelines Section 15070, a Negative Declaration (ND) or MND shall be prepared for a project subject to CEQA when either:

- a) The IS shows that there is no substantial evidence, in light of the whole record before the agency, that the project may have a significant effect on the environment, or
- b) The IS identifies potentially significant effects, but:
 - 1) Revisions in the project plans or proposals made by, or agreed to by the applicant before the proposed MND and IS are released for public review would avoid the effects or mitigate the effects to a point where clearly no significant effects would occur, and
 - 2) There is no substantial evidence, in light of the whole record before the agency, that the project as revised may have a significant effect on the environment.

This IS/MND has been prepared in accordance with the CEQA, Public Resources Code Section 21000 et seq., and the State CEQA Guidelines Title 14 California Code of Regulations (CCR) Section 15000 et seq.

1.2 LEAD AGENCY

The lead agency is the public agency with primary responsibility over a proposed project. Where two or more public agencies will be involved with a project, CEQA Guidelines Section 15051 provides criteria for identifying the lead agency. In accordance with CEQA Guidelines Section 15051(b)(1), "The lead agency will normally be the agency with general governmental powers rather than an agency with a single or limited purpose." In addition, Section 15051(c) states "where more than one public agency equally meet the criteria in subdivision (b), the agency which will act first on the project in question shall be the lead agency". The City Public Works Department has initiated preliminary design of the project. The Project lies within the City limits of the City of Lodi and requires approval from the City of Lodi City Council. Therefore, based on the criteria described above, the lead agency for the proposed project is the City of Lodi, Public Works Department.

1.3 PURPOSE AND DOCUMENT ORGANIZATION

The purpose of this Initial Study and proposed Negative Declaration (IS/ND) is to identify the potential environmental impacts and mitigation measures associated with the proposed Harney Lane Specific Plan, which seeks to reach a consensus of the most appropriate roadway improvements to be implemented in the Harney Lane corridor to accommodate anticipated growth and traffic volume increases and establish the necessary right of way needed to be acquired, reserved and/or dedicated in order to accommodate the roadway improvements. Pursuant to Section 15367 of the CEQA Guidelines, the City is the Lead Agency in the preparation of this IS/ND, and any additional environmental documentation required for the project. The intended use of this document is to provide information to support conclusions regarding the potential environmental impacts of the project. The IS/ND provides the basis for input from public agencies, organizations, and interested members of the public.

The proposed Mitigated Negative Declaration tiers of the City of Lodi General Plan 2010 and General Plan EIR 2010 (SCH#2009022075), which serve as the project's program level EIR. The proposed Harney Lane Specific Plan intends to refine the vision, goals, policies, and actions of the City's General Plan by establishing area-specific goals and policies to guide land use patterns in the Specific Plan Area (along Harney Lane). The proposed Harney Lane Specific Plan involves land that is incorporated into, and planned for development in Lodi's 2010 General Plan. Having been so included, all General Plan level environmental effects were of necessity, therein addressed. As a tiered document, the Initial Study/Negative Declaration for the project relies, in part, on the General Plan 2010 and General Plan EIR 2010, for:

- 1) A discussion of general background and setting information for environmental topic areas;
- 2) Overall growth-related issues, land uses, level of service related to traffic;
- 3) Issues that were evaluated in sufficient detail in the 2010 General Plan EIR, for which there are no significant new information or changes in circumstances that would require further analysis; and
- 4) Analysis of long-term cumulative impacts.

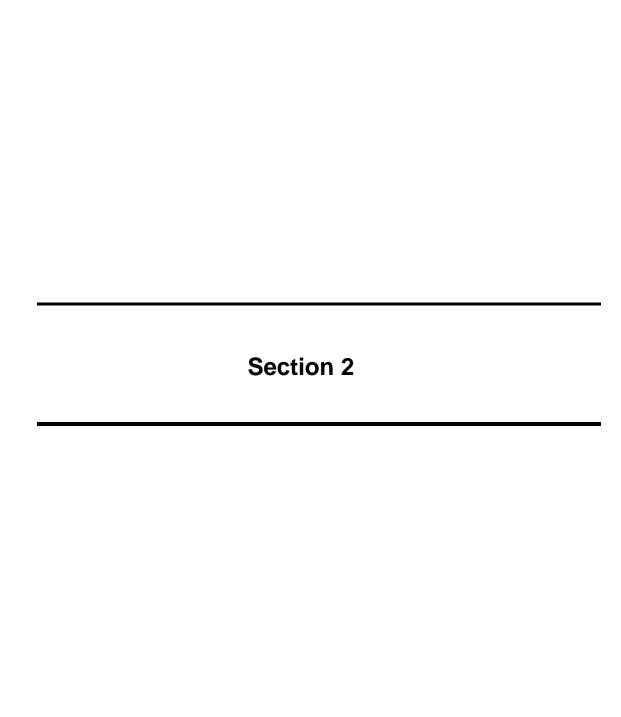
This Tiered Initial Study/Negative Declaration analyzes the potential site-specific and localized impacts of the Project. As the analysis demonstrates, there are no new significant impacts identified due to the project since no physical improvements or construction activities are proposed by the project itself at this time. The Harney Lane Specific Plan serves as an implementing arm of the City's General Plan; therefore, the General Plan EIR has assessed many project related impacts. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis. Since specific development outcomes and impacts cannot be accurately assessed at this time, this document will discuss the general impacts imposed by the Project. Because there are no new significant impacts identified there are no new alternatives to the project that need be examined and therefore, the previous analysis is sufficient. Additionally, because there are no new significant impacts identified, the cumulative impacts remain the same. Thus, the information contained in this

subsequent Negative Declaration is sufficient to meet the requirements of CEQA Guidelines Section 15163.

1.4 INCORPORATION BY REFERENCE

The references outlined below were utilized during preparation of this Initial Study/Negative Declaration. The documents are available for review at the City of Lodi, Community Development Department, located at 221 West Pine Street, California 95240.

- <u>City of Lodi General Plan 2010.</u> State law requires every city and county to adopt a comprehensive, long-term general plan for the physical development of that city and county. The City of Lodi *General Plan*, adopted April 2010, contains goals, policies, and programs which are intended to guide land use and development decisions for the next twenty years. The *General Plan* consists of eight elements, or chapters, which together fulfill the requirements for a general plan. The *General Plan* chapter include the Land Use; Growth Management and Infrastructure; Community Design and Livability; Transportation; Parks, Recreation and Open Space; Conservation; Safety, and Noise Elements.
- <u>City of Lodi General Plan Final Environmental Impact Report, February 2010</u>. The City of Lodi General Plan, *Final Environmental Impact Report (General Plan FEIR), SCH2009022075*, is intended to provide information to public agencies and the general public regarding the potential environmental impacts related to implementation of the City of Lodi General Plan. The purpose of the EIR is "to identify the significant effects of a project on the environment, to identify alternatives to the project and to indicate the manner in which significant impacts can be mitigated or avoided."
- <u>City of Lodi General Plan Draft Environmental Impact Report, November 2009.</u> The City of Lodi, *Pubic Review Draft General Plan Environmental Impact Report, SCH*2009022075, is a first-tier evaluation of the environmental effects associated with the adoption of the updated City of Lodi General Plan.
- <u>City of Lodi Municipal Code</u>. The City of Lodi *Zoning Code* is contained in Chapter 17 of the Lodi Municipal Code (LMC) and represents the minimum requirement for the promotion of public safety, health, convenience, comfort, prosperity or general welfare.



2.1 PROJECT BACKGROUND

The City of Lodi adopted its current General Plan in April of 2010. The General Plan is the City's vision for how to accommodate anticipated growth within the next 20 to 30 years. As part of the General Plan, the City reviews the forecasted traffic volumes based on the anticipated growth of the city. The number of lanes for each of the roads in the network is determined from these forecasts. A minimum Level of Service (LOS) "E" is maintained throughout the City. Chapter 5 of the General Plan designates Harney Lane as a four lane expressway from Lower Sacramento Road to State Route 99. To achieve this goal, the City proposes Harney Lane Specific Plan, which intends to refine the vision, goals, policies, and actions of the City's General Plan by establishing area-specific goals and policies to guide land use patterns in the Specific Plan Area (along Harney Lane).

2.2 PROJECT LOCATION

The projects limits are located on Harney Lane, between Stockton Street and Lower Sacramento Road in the City of Lodi, in San Joaquin County. Harney Lane, as illustrated in Exhibit 1, is located at the southern edge of the current City limits. Regional access to the project limits is from SR Route 99 via Harney Lane exit. Exhibit 1 shows the project's location from a local and regional context.

2.3 PROJECT PURPOSE AND OBJECTIVES

The purpose of the Harney Lane Specific Plan is to reach a consensus of the most appropriate roadway improvements to be implemented in the Harney Lane corridor to accommodate anticipated growth and traffic volume increases and establish the necessary right of way needed to be acquired, reserved and/or dedicated in order to accommodate the roadway improvements. Harney Lane is planned to be one of the main east-west connectors across the south side of the city. According to the General Plan, Harney Lane is classified as a minor arterial west of Lower Sacramento Road and an expressway from Lower Sacramento Road to State Route 99. Lower Sacramento Road is also designated an expressway (the main north-south connector on the west side of the city) connecting Harney Lane to Kettleman Lane

2.4 PROJECT DESCRIPTION

The proposed Harney Lane Specific Plan involves widening of Harney Lane so as to relieve existing and future traffic congestion at the west-east bound road. Harney Lane currently is a two lane road with a posted speed of 45 mph. North of Harney Lane is mostly new single family residential subdivisions with block walls fronting Harney Lane. Harney Lane has been widened to the ultimate width on the north where the recent development has occurred. The south side is a mixture of agriculture (grapes and cherries) and single family residences. West of Lower Sacramento Road single family

residences populate both sides of the road. Between South Hutchins Street and South Stockton Street, Harney Lane crosses the Union Pacific Railroad. The proposed Harney Lane Specific Plan mostly affects the south side of Harney Lane.

The proposed Harney Lane Specific Plan is an integral component of the City's General Plan 2010, which identifies Harney Lane as a four lane express way. The proposed Harney Lane Specific Plan details roadway improvements, establish the necessary right-of-way required to accommodate the roadway improvements and it affects a total of 47 privately-owned parcels. Proposed right-of-way acquisitions would occur in conjunction with actual construction projects. No physical improvements or construction activities are proposed in conjunction with implementation of the Harney Lane Specific Plan. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

The project design, as illiterate in Exhibit 5, was prepared in accordance with the City framework set out in the General Plan adopted on April 7, 2010. Items considered during the preparation of the Specific Plan include future land use, roadway and bicycle network, right of way issues, safety (pedestrian and vehicular), ingress and egress for residents and businesses and environmental issues. There are no physical improvements or construction activities proposed by the project itself at this time.

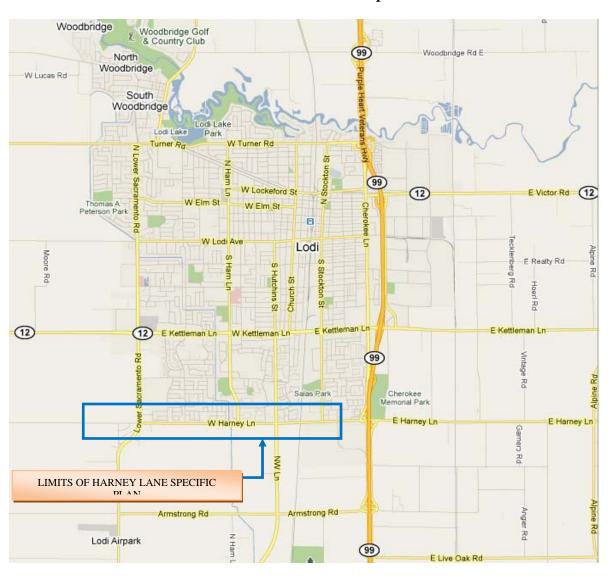
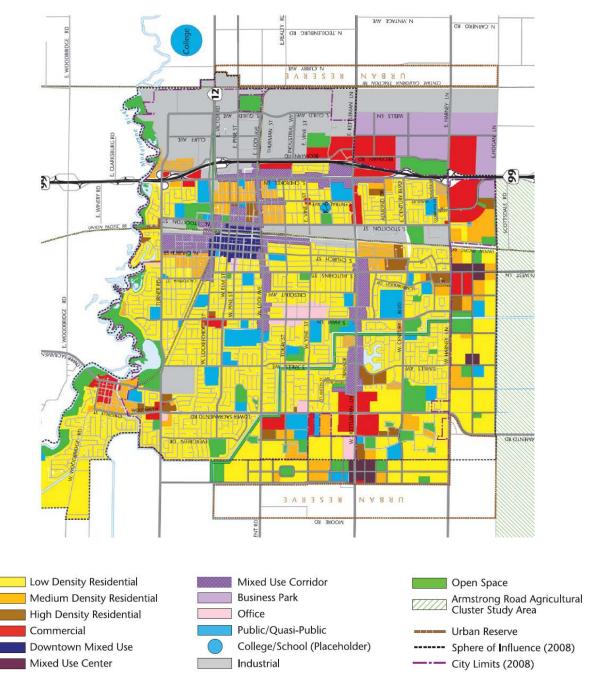


Exhibit 1: Location Map

Exhibit 2: Existing Land Use Diagram

FIGURE 2-1: LAND USE DIAGRAM



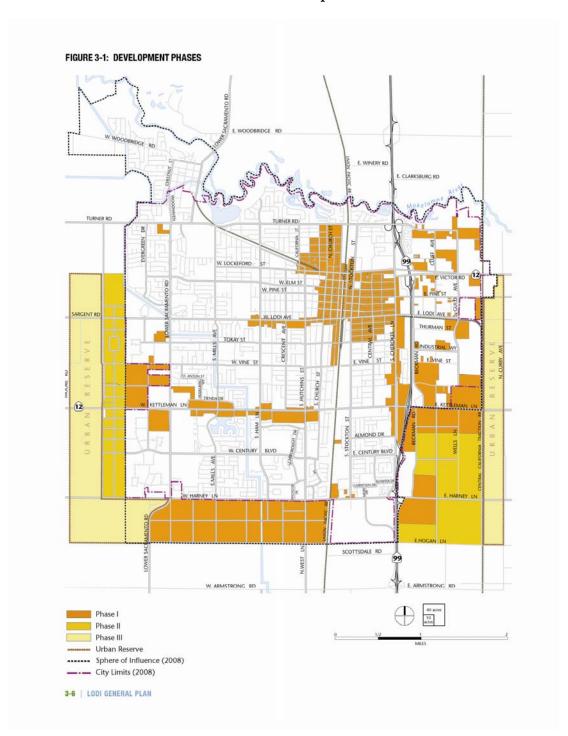
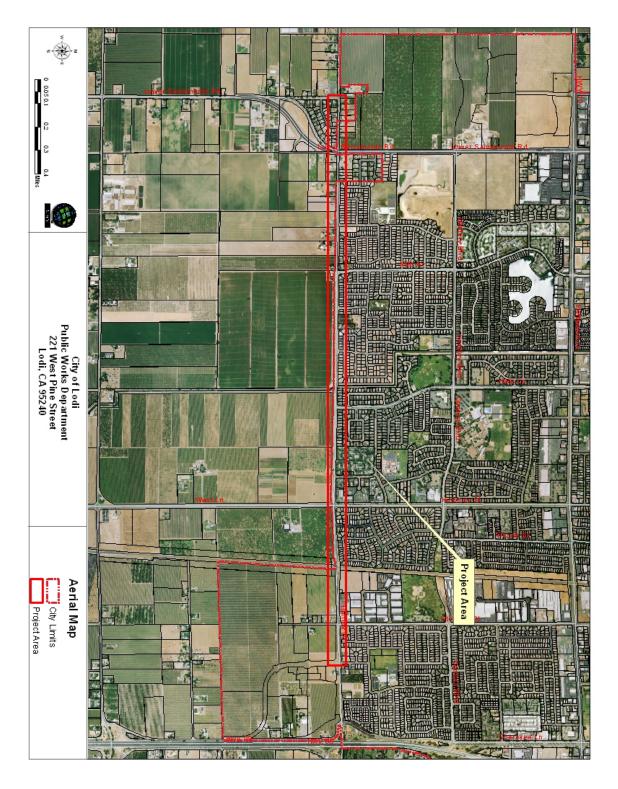


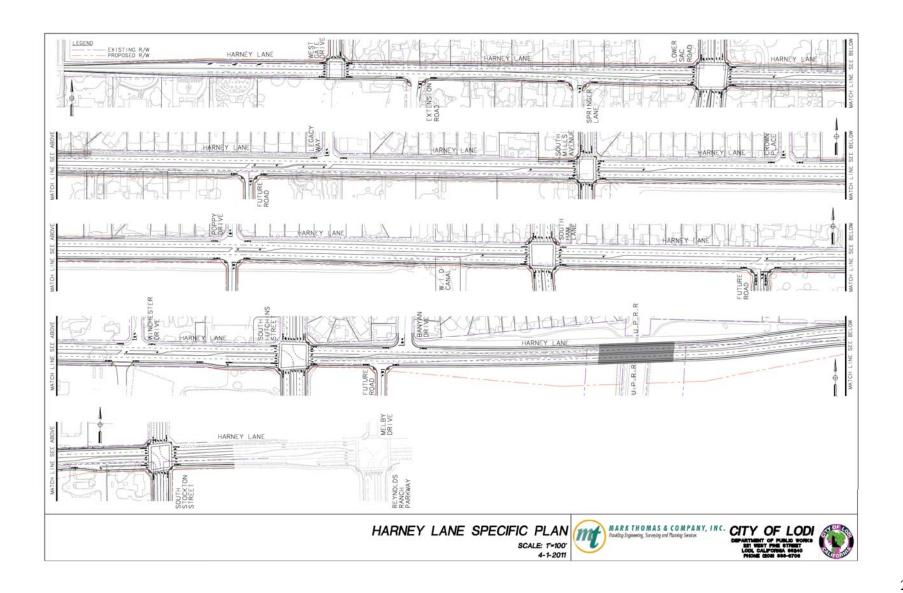
Exhibit 3: Development Phases

Exhibit 4: Aerial Map



2.0 PROJECT DESCRIPTION

Exhibit 5: Harney Lane Specific Plan Design



2.5 LEAD AGENCY NAME AND ADDRESS:

City of Lodi, Public Works Department 221 West Pine Street Lodi, CA 9540

2.6 CONTACT PERSONS AND PHONE NUMBER:

Environmental document: Manny Bereket 209-333-6711
Project Coordinators: Wes Fujitani 209-333-6706
Chris Boyer 209-333-6706

2.7 PROJECT SPONSOR'S NAME AND ADDRESS:

City of Lodi Public Works Department 221 W. Pine Street Lodi CA 95240

2.8 GENERAL PLAN DESIGNATIONS:

The parcels fronting on north side of Harney Lane (within incorporated City limits) have a mixture of General Plan designations of Industrial, Commercial, Single Family Residence, Medium Density Residences, and High Density Residences.

The parcels fronting on the south side of Harney Lane are within the San Joaquin County jurisdiction and have a General Plan Designation of Ag (Agricultural Land) and R/VL – Residential.

2.9 ZONING DESIGNATIONS:

The parcels fronting on north side of Harney Lane (within incorporated City limits) have a mixture of Zoning designations of M-2, Heavy Industrial, C-1, Neighborhood Commercial, R-2, Single Family Residence, and PD- Planned Development Units.

The parcels fronting on the south side of Harney Lane are within the San Joaquin County jurisdiction and have a variety of Zoning Designations of Ag-40 (Agricultural Land – minimum of 40 acres) and R/VL – Residential.

2.10 PROJECT CONSTRUCTION

The proposed Harney Lane Specific Plan involves widening of Harney Lane so as to relieve existing and future traffic congestion at the west-east bound road. Harney Lane currently is a two lane road with a posted speed of 45 mph. North of Harney Lane is mostly new single family residential subdivisions with block walls fronting Harney Lane. Harney Lane has been widened to the ultimate width on the north where the recent development has occurred. The south side is a mixture of agriculture (grapes and cherries) and single family residences. West of Lower Sacramento Road single family residences populate both sides of the road. Between South Hutchins Street and South Stockton Street, Harney Lane crosses the Union

Pacific Railroad. The proposed Harney Lane Specific Plan mostly affects the south side of Harney Lane.

The proposed Harney Lane Specific Plan is an integral component of the City's General Plan 2010, which identifies Harney Lane as a four lane express way. The proposed Harney Lane Specific Plan details roadway improvements, establish the necessary right-of-way required to accommodate the roadway improvements and it affects a total of 47 privately-owned parcels. Proposed right-of-way acquisitions would occur in conjunction with actual construction projects. No physical improvements or construction activities are proposed in conjunction with implementation of the Harney Lane Specific Plan. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

The project design, as illiterate in Exhibit 5, was prepared in accordance with the City framework set out in the General Plan adopted on April 7, 2010. Items considered during the preparation of the Specific Plan include future land use, roadway and bicycle network, right of way issues, safety (pedestrian and vehicular), ingress and egress for residents and businesses and environmental issues. There are no physical improvements or construction activities proposed by the project itself at this time.

2.11 REQUIRED PROJECT APPROVALS

In order for the project to be implemented, a series of actions and approvals would be required from agencies. Anticipated project approvals/actions would include, but are not limited to the following:

• Lodi City Council - Adoption of the circulated ND, and actions associated with Harney Lane Specific Plan.

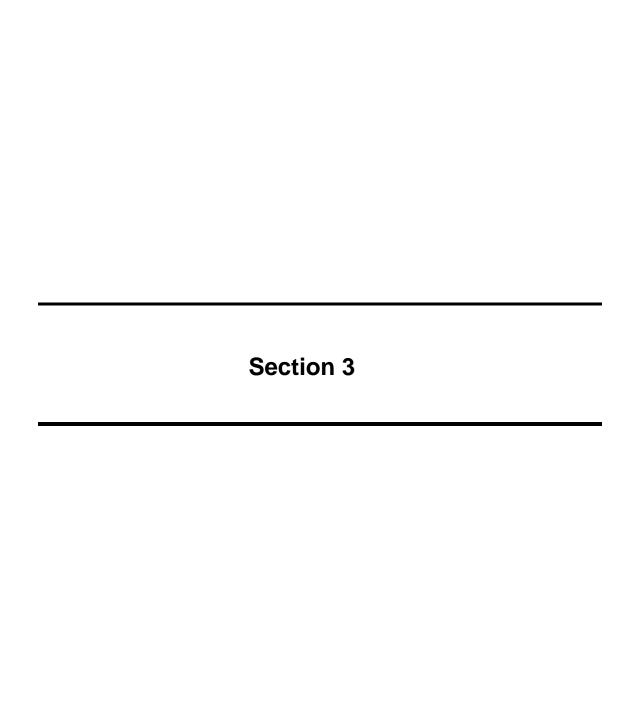
2.12 OTHER PROJECT ASSUMPTIONS

This IS/ND assumes compliance with all applicable state, federal, and local codes and regulations including, but not limited to, City of Lodi Standards, the Guidance Manual for On-site Storm Water Quality Control Measures, the State Health and Safety Code, and the State Public Resources Code.

2.13 TECHNICAL STUDIES

The following technical study forms the basis of this IS/ND:

• Draft Harney Lane Specific Plan, dated January 10, 2011. Prepared by Mark Thomas and Company, Inc.,



3.1 ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project as indicated by the checklist on the following pages.

| Env | rironmental Factors Potentially | Environmental Factors Potentially Affected | | | | | | | | |
|-------------|---|--|----------------------------------|-------------|-----------------------------|--|--|--|--|--|
| | Aesthetics | | Agriculture Resources | | Air Quality | | | | | |
| | Greenhouse Gas Emissions | | Biological Resources | | Cultural Resources | | | | | |
| | Geology/Soils | | Hazards & Hazardous Materials | | Hydrology/Water Quality | | | | | |
| | Land Use/Planning | | Mineral Resources | | Noise | | | | | |
| | Population/Housing | | Public Services | | Recreation | | | | | |
| | Transportation/Traffic | | Utilities/Services Systems | s | | | | | | |
| | Mandatory Findings of Significan | nce | | | | | | | | |
| ' 2 2 E | NIVIDONIEMNITAI DETEDA | TTNT A | CIONI. On the begin of this | . ! ! 4 ! . | | | | | | |
| 3.2 E □ | NVIRONEMNTAL DETERM | | | | | | | | | |
| | I find that the proposed project | | | tect o | on the environment, and a | | | | | |
| | Negative Declaration will be p | - | | ont a | offeet on the environment | | | | | |
| \boxtimes | I find that although the propo | _ | , | | | | | | | |
| | there will not be a significant | | | | - , | | | | | |
| | made by or agreed to by the prepared. | projec | t proponent. A Mitigated | ı ne | gative Declaration will be | | | | | |
| | I find that the proposed proje | oct M | AV havo a cignificant offoc | t on | the environment and an | | | | | |
| | Environmental Impact Report | | _ | . ()11 | the environment, and an | | | | | |
| | I find that the proposed proje | - | | ifica | nt impact" or "notontially | | | | | |
| | significant unless mitigated" | | | | | | | | | |
| | adequately analyzed in an ea | _ | | | · | | | | | |
| | - , , | | - | | | | | | | |
| | has been addressed by mitig | | | | • | | | | | |
| | attached sheets. An Environm | | impact Report is required | ı, bu | t it must analyze omy the | | | | | |
| | effects that remain to be addre | | uningt and 1d leave a signific | | effect on the consideration | | | | | |
| Ш | I find that although the propo | _ | , | | | | | | | |
| | because all potentially signific | | • | | | | | | | |
| | or Negative Declaration pursuant to applicable standards, and (b) have been avoided | | | | | | | | | |
| | mitigated pursuant to that earlier EIR or Negative Declaration, including revisior | | | | | | | | | |
| | mitigation measures that are is | mpose | d upon the proposed proje | ect, n | othing further is required. | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| Konr | adt Bartlam, Community Develop | oment | Director Date | | | | | | | |

NOTICE OF AVAILABILITY

Notice is herby given that the City of Lodi, Community Development Department, has completed an initial study and proposed a Negative Declaration pursuant to the California Environmental Quality Act for the project described below.

The initial study prepared by the City was undertaken for the purpose of determining whether the proposed Harney Lane Specific Plan may have a significant effect on the environment. On the basis of the initial study, Community Development Department staff has concluded that the proposed Harney Lane Specific Plan will not have a significant effect on the environment, and therefore has prepared a proposed Negative Declaration 11-MND-01. The initial study reflects the independent judgment of the City.

FILE NUMBER: 11-ND-01

PROJECT TITLE: Harney Lane Specific Plan

PROJECT DESCRIPTION: The proposed Harney Lane Specific Plan involves widening of Harney Lane so as to relieve existing and future traffic congestion at the west-east bound road. Harney Lane currently is a two lane road with a posted speed of 45 mph. North of Harney Lane is mostly new single family residential subdivisions with block walls fronting Harney Lane. Harney Lane has been widened to the ultimate width on the north where the recent development has occurred. The south side is a mixture of agriculture (grapes and cherries) and single family residences. West of Lower Sacramento Road single family residences populate both sides of the road. Between South Hutchins Street and South Stockton Street, Harney Lane crosses the Union Pacific Railroad. The proposed Harney Lane Specific Plan mostly affects the south side of Harney Lane.

The proposed Harney Lane Specific Plan is an integral component of the City's General Plan 2010, which identifies Harney Lane as a four lane express way. The proposed Harney Lane Specific Plan details roadway improvements, establish the necessary right-of-way required to accommodate the roadway improvements and it affects a total of 47 privately-owned parcels. Proposed right-of-way acquisitions would occur in conjunction with actual construction projects. No physical improvements or construction activities are proposed in conjunction with implementation of the Harney Lane Specific Plan. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

The project design, as illiterate in Exhibit 5, was prepared in accordance with the City framework set out in the General Plan adopted on April 7, 2010. Items considered during the preparation of the Specific Plan include future land use, roadway and bicycle network, right of way issues, safety (pedestrian and vehicular), ingress and egress for residents and businesses and environmental issues. There are no physical improvements or construction activities proposed by the project itself at this time.

PROJECT LOCATION: The projects limits are located on Harney Lane, between Stockton Street and Lower Sacramento Road in the City of Lodi, in San Joaquin County. Harney Lane,

as illustrated in Exhibit 1, is located at the southern edge of the current City limits. Regional access to the project limits is from SR Route 99 via Harney Lane exit. Exhibit 1 shows the project's location from a local and regional context.

PUBLIC REVIEW PERIOD: The proposed Negative Declaration will be circulated for a 30-day public review period, beginning on **Tuesday**, **April 12**, **2011** and ending on **Wednesday**, **May 11**, **2011**. Copies of the document are available for review at the following locations:

- Community Development Department, 221 West Pine Street, Lodi, CA 95240
- Lodi Public Library, 201 West Locust Street, Lodi, CA 95240
- Public Works Department, 221 West Pine Street, Lodi, CA 95240

The Mitigated Negative Declaration is also available for review on the internet at the following web address: http://www.lodi.gov/com_dev/EIRs.html

Any person wishing to comment on the Initial Study and proposed Negative Declaration must submit such comments in writing **no later than 5:00 pm on Wednesday May 11, 2011**to the City of Lodi at the following address:

Community Development Director City of Lodi P. O. Box 3006 Lodi, CA 95241

Facsimiles at (209) 333-6842 will also be accepted up to the comment deadline (please mail the original). For further information, contact Immanuel Bereket, Associate Planner, at (209)333-6711.

Konradt Bartlam, Community Development Director City of Lodi P. O. Box 3006 Lodi, CA 95241

The City will provide additional public notices when the public hearings have been scheduled to consider approval of the proposed Negative Declaration and the other entitlements for the project.

PROPOSED NEGATIVE DECLARATION

Prepared pursuant to City of Lodi Environmental Guidelines, §§ 1.7 (c), 5.5

FILE NUMBER: 11-MND-01

PROJECT TITLE: Harney Lane Specific Plan

PROJECT DESCRIPTION: The proposed Harney Lane Specific Plan involves widening of Harney Lane so as to relieve existing and future traffic congestion at the west-east bound road. Harney Lane currently is a two lane road with a posted speed of 45 mph. North of Harney Lane is mostly new single family residential subdivisions with block walls fronting Harney Lane. Harney Lane has been widened to the ultimate width on the north where the recent development has occurred. The south side is a mixture of agriculture (grapes and cherries) and single family residences. West of Lower Sacramento Road single family residences populate both sides of the road. Between South Hutchins Street and South Stockton Street, Harney Lane crosses the Union Pacific Railroad. The proposed Harney Lane Specific Plan mostly affects the south side of Harney Lane.

The proposed Harney Lane Specific Plan is an integral component of the City's General Plan 2010, which identifies Harney Lane as a four lane express way. The proposed Harney Lane Specific Plan details roadway improvements, establish the necessary right-of-way required to accommodate the roadway improvements and it affects a total of 47 privately-owned parcels. Proposed right-of-way acquisitions would occur in conjunction with actual construction projects. No physical improvements or construction activities are proposed in conjunction with implementation of the Harney Lane Specific Plan. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

The project design, as illiterate in Exhibit 5, was prepared in accordance with the City framework set out in the General Plan adopted on April 7, 2010. Items considered during the preparation of the Specific Plan include future land use, roadway and bicycle network, right of way issues, safety (pedestrian and vehicular), ingress and egress for residents and businesses and environmental issues. There are no physical improvements or construction activities proposed by the project itself at this time.

PROJECT LOCATION: The projects limits are located on Harney Lane, between Stockton Street and Lower Sacramento Road in the City of Lodi, in San Joaquin County. Harney Lane, as illustrated in Exhibit 1, is located at the southern edge of the current City limits. Regional access to the project limits is from SR Route 99 via Harney Lane exit. Exhibit 1 shows the project's location from a local and regional context.

NAME OF PROJECT PROPONENT/APPLICANT:

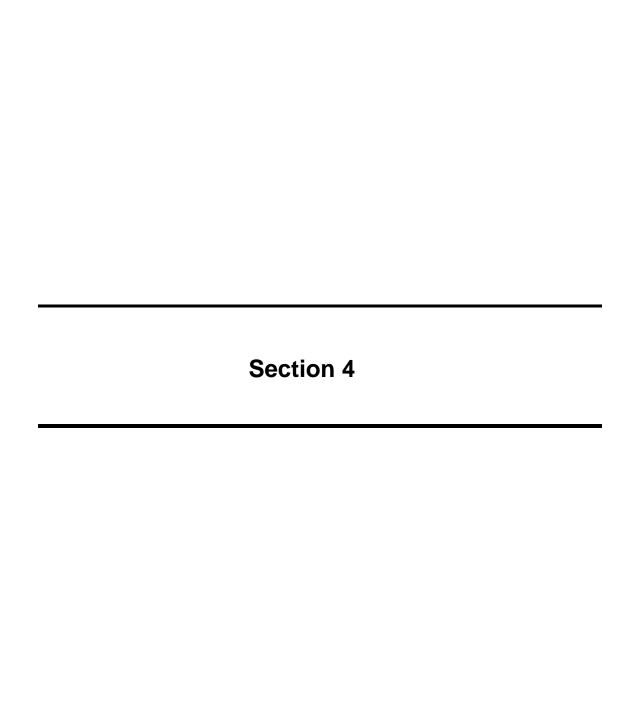
City of Lodi , Public Works Department 221 West Pine Street Lodi, CA 95240 A copy of the Initial Study ("Environmental Information Form" and "Environment Checklist") documenting the reasons to support the adoption of a Negative Declaration is available at the City of Lodi Community Development Department.

Mitigation measures are \boxtimes are not \square included in the project to avoid potentially significant effects on the environment.

The public review on the proposed Negative Declaration will commence on **Tuesday**, **April 12**, **2011** and ending on **Wednesday**, **May 11**, **2011**.

| The | City | will | provide | additional | public | notices | when | the | public | hearings | have | been |
|------|---|------|---------|------------|--------|---------|------|-----|--------|----------|------|------|
| sche | scheduled to consider approval of the Negative Declaration. | | | | | | | | | | | |

| Konradt Bartlam, Community Development Director | Date |
|---|------|



| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|----|---|--------------------------------------|---|-------------------------------------|--------------|
| 4.1 | | STHETICS . uld the Project: | | | | |
| | a. | Have a substantial adverse effect on a scenic vista? | | | | • |
| | b. | Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? | | | | • |
| | c. | Substantially degrade the existing visual character or quality of the site and its surroundings? | | | | • |
| | d. | Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area? | | | • | |

As indicated in the project description, the proposed Harney Lane Specific Plan is an integral part of the City's General Plan 2010 and involves establishment of the necessary right-of-way required to accommodate the roadway improvements. Proposed right-of-way acquisitions would occur in conjunction with actual construction projects. No physical improvements or construction activities are proposed in conjunction with implementation of the Harney Lane Specific Plan. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

Impact Analysis:

(a) Determination of significance for potential impacts to visual resources is based primarily on the level of visual sensitivity in an area. Scenic vistas typically consist of a far reaching view, such as a panoramic view of a skyline or ridgeline, and provide an aesthetic public benefit (i.e. available to the general public). All roads nationally designated as such are considered part of America's Byways collection and must possess at least one of these six intrinsic qualities: historic, cultural, natural, scenic, recreational, and/or archaeological. To receive an All-American Road designation, a road must possess multiple intrinsic qualities that are nationally significant and contain one-of-a-kind features that do not exist elsewhere. The road must also be considered a "destination unto itself," and must provide an exceptional travel experience. (http://www.scenic.org/byways).

The San Joaquin County General Plan and the City of Lodi General Plan do not designate specific areas within the Project site as scenic vistas, and the views from the project site consist of agricultural lands to the south and existing residential and commercial developments to the north, and ornamental landscaping. The topography in the project area is generally flat and does not support far reaching views. Further, the Harney Lane Specific Plan does not involve construction, site grading, and disturbing.

Future construction project would be viewed for potential environmental impact on project basis. Therefore, no impact would occur in this regard.

Significance Determination: No impact.

Mitigation Measures: Mitigation measures are not required.

Significance After Mitigation: No impact.

(b) The Harney Lane Specific Plan would not affect a scenic vista or scenic resources scenic the project does not involve physical improvements or construction activities. No state scenic highways are located within the project area at this time and none of the local roads within the project area have been designated as scenic (State Routes 12 and 99 are not designated as scenic within or abutting the project area). Therefore, no impact would result associated with scenic resources visible from a designated scenic highway.

Significance Determination: No impact.

Mitigation Measures: Mitigation measures are not required.

Significance After Mitigation: No impact.

(c) A project is generally considered to have a significant aesthetic impact if the project substantially changes the character of the project site such that it becomes visually incompatible in comparison to that of its surroundings.

The project site is located within a partially urbanized area of the City and mostly agricultural land within the County. The project site consists of an existing roadway, surrounded by residential and commercial development. The project site and the surrounding area are not recognized as scenic resources or contain structures that have unique architectural styles or historical significance. Further, Harney Lane Specific Plan does not involve physical improvements or construction activities. Therefore, no impact would occur.

Significance Determination: No impact.

Mitigation Measures: Mitigation measures are not required.

Significance After Mitigation: No impact.

(d) Currently, the primary source of day and nighttime lighting and glare in the area is from Harney Lane and urban development around the project site. The main sources of daytime glare in the area are from sunlight reflecting from structures with reflective surfaces such as windows. Building materials (i.e., reflective glass and polished surfaces) are the most substantial sources of glare. The amount of glare depends on the intensity and direction of sunlight, which is more acute at sunrise and sunset because the angle of the sun is lower during these times. Nighttime light sources include, but are not limited to, residential developments, vehicles (headlights), overhead street lighting, parking lot lighting, and security related lighting for non-residential uses.

Implementation of Harney Lane Specific Plan would require the replacement existing streetlights and installation of new streetlights. However, new streetlights would be controlled through the existing City Code, which requires street lights to be directed down and shielded away from adjacent properties (Chapter 16.24). Nuisance lighting is regulated by the Municipal Code § 17.81.050. Nevertheless, the proposed Harney Lane Specific Plan does not propose construction plans or installation of streetlights. Therefore, no impact would occur.

Significance Determination: No impact.

Mitigation Measures: Mitigation measures are not required.

Significance After Mitigation: No impact.

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|-------------------------------------|--|--------------------------------------|---|-------------------------------------|--------------|
| 4.2 | In a sign Call Ass of C | RICULTURE RESOURCES: letermining whether impacts to agricultural resources are ificant environmental effects, lead agencies may refer to the ifornia Agricultural Land Evaluation and Site essment Model (1997) prepared by the California Dept. Conservation as an optional model to use in assessing acts on agriculture and farmland. Would the Project: | | | | |
| | a. | Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program in the California Resources Agency, to non-agricultural use? | | | | • |
| | b. | Conflict with existing zoning for agricultural use, or a Williamson Act contract? | | | | • |
| | c. | Conflict with existing zoning for, or cause rezoning of forest land (as defined in PRC Sec. 4526), or timberland zoned Timberland Production (as defined in PRC Sec. 51104 (g)? | | | | • |
| | d. | Result in loss of forest land or conversion of forest land to non-forest use? | | | | |
| | e. | Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use? | | | | • |

The project occurs partially in agricultural fields and partially in fully developed urban area that does not contain any agricultural farmland. No parts of the project limits include forest uses. As indicated in the project description, the proposed Harney Lane Specific Plan is an integral part of the City's General Plan 2010 and involves establishment of the necessary right-of-way required to accommodate the roadway improvements. Proposed right-of-way acquisitions would occur in conjunction with actual construction projects. No physical improvements or construction activities are proposed in conjunction with implementation of the Harney Lane Specific Plan. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

(a) Agriculture has historically been an important part of Lodi's land use and economy. Impacts resulting from conversion of important farmland, including conversions for transportation improvements, were considered and analyzed in the City's General Plan EIR (2009). In addition, the City's General Plan policies C-P7 and C-P8 involve mitigation measures aimed for the preservation of agricultural land and activities. The proposed Harney Lane Specific Plan is an implementing arm of the said General Plan and involves no construction activities. Future construction projects would be subjected

to environmental review on a project-by-project basis. Because the proposed Harney Lane Specific Plan would not convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, the project would have no impact from conversion of farmland.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(b) The proposed Harney Lane Specific Plan does not involve physical improvements or construction activities. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis. Therefore, no impact would occur due to the proposed Harney Lane Specific Plan.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(c) A significant impact may occur if the proposed Project were to result in the conversion of forest land to non-forest land.

There is no existing zoning for forest land on or near the project limits. The proposed Harney Lane Specific Plan does not involve physical improvements or construction activities. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis. Therefore, no impact would occur due to the proposed Harney Lane Specific Plan.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(d) A significant impact may occur if the proposed Project were to result in the conversion of forest land to non-forest land.

There is no forest land located in or around the project limits; therefore no impact to these resources would occur.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(e) A significant impact may occur if it involves changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use. The proposed Harney Lane Specific Plan does not involve physical improvements or construction activities. Subsequent development in the Plan Area,

including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis. Therefore, no impact would occur due to the proposed Harney Lane Specific Plan.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|----|--|--------------------------------------|---|-------------------------------------|--------------|
| 4.3 | | QUALITY. Id the Project: | | | | |
| | a. | Conflict with or obstruct implementation of the applicable air quality plan? | | | | • |
| | b. | Violate any air quality standard or contribute substantially to an existing or Projected air quality violation? | | | | • |
| | c. | Result in a cumulatively considerable net increase of any criteria pollutant for which the Project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? | | | | • |
| | d. | Expose sensitive receptors to substantial pollutant concentrations? | | | | • |
| | e. | Create objectionable odors affecting a substantial number of people? | | | | • |

(a) The Federal Clean Air Act established Federal air quality standards known as the National Ambient Air Quality Standards. These standards identify levels of air quality for "criteria" pollutants (Ozone $[0_3]$, Carbon Monoxide [CO], Nitrogen Oxides $[NO_x]$, Sulfur Oxides $[SO_x]$, Particulate Matter $[PM_{10}]$, Fine Particulate Matter $[PM_{2.5}]$:, and Lead [Pb]) that are considered the maximum levels of ambient (background) air pollutants considered safe, with an adequate margin of safety, to protect the public health and welfare.

The City of Lodi is located in the San Joaquin Valley Air Basin (SJVAB). Air quality conditions within the SJVAB are under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). SJVAPCD does not require construction emissions to be quantified. Rather, it requires implementation of effective and comprehensive feasible control measures to reduce PM₁₀ emissions (San Joaquin Valley Air Pollution Control District 2002). SJVAPCD considers PM₁₀ emissions to be the greatest pollutant of concern when assessing construction-related air quality impacts. It has determined that compliance with its Regulation VIII, including implementation of all feasible control measures specified in its *Guide for Assessing Air Quality Impacts* (San Joaquin Valley Air Pollution Control District 2002) constitutes sufficient mitigation to reduce construction-related PM₁₀ emissions to less-than-significant levels and minimize adverse air quality effects.

The proposed Harney Lane Specific Plan does not involve construction activities. Air Quality impacts have been exhaustively examined and mitigation measures have been detailed in the City's General Plan EIR (SCH2009022075) and mitigation polices are incorporated in the General Plan policy. Future projects and developments in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis. Therefore, no impact would occur due to the proposed Harney Lane Specific Plan.

- 1. The project would not result in short-term construction emissions that would exceed the CEQA significance emissions thresholds established by the SJVAPCD and there would be no long-term emissions associated with the project as it involves no construction activities.
- 2. The project would not affect growth forecasts in the Air Quality Management Plan, since it does not propose physical improvements or construction activities.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(b) As aforementioned in item (a), the proposed project will not violate any air quality standard or contribute substantially to an existing or projected air quality violation since it does not involve physical improvements or construction activities. All future projects including, but not limited to, Subdivision Maps, Parcel Maps, Conditional Use Permits, Site Plan Review, and Planned Development Review projects must be evaluated to ensure compliance with air quality standards, including construction, area source, and operational emissions.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(c) As discussed in checklist item 4.3(a) and(b), the project will not significantly increase the production of any criteria pollutant as described in section a), therefore, it is appropriate to conclude that the project's incremental contribution to criteria pollutant emissions is not cumulatively considerable. Future construction activities will be subject to environmental review on a project-by-project basis.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(d) When quantifying mass emissions for localized analysis, only emissions that occur onsite are considered. Consistent with SJVAPCD Localized Significance Threshold (LST) methodology guidelines, emissions related to off-site delivery/haul truck activity and employee trips are not considered in the evaluation of localized impacts. As such, localized impacts that may result from Harney Lane Specific Plan would be of no consequences as there no construction activity is being proposed at this time.

Sensitive receptors near the project site include the nearby residences located along north of Harney Lane and interspersed along the south side of Harney Lane, and commercial properties along Harney Lane. However, as previously mentioned, The proposed Harney Lane Specific Plan does not involve construction activities. Air Quality impacts have been exhaustively examined and mitigation measures have been detailed in the City's General Plan EIR (SCH2009022075) and mitigation polices are incorporated in the General Plan policy. Future projects and developments in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis. Therefore, the proposed project would not exceed any of the established air quality thresholds. There will be no impacts resulting from the proposed project and would not expose sensitive receptors to substantial pollutant concentrations.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(e) According to the SJVAPCD *Guide For Assessing and Mitigating Air Quality Impacts,* land uses associated with odor complaints typically include agricultural uses, wastewater treatment plants, food processing plants, chemical plants, composting, refineries, landfills, dairies, and fiberglass molding facilities. The proposed project does not include any uses identified by SJVAPCD as being associated with odors. Further, no construction activities or materials are proposed that would as part of the Harney Lane Specific Plan. As such, no potential odor impacts are anticipated due to the project.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|----|---|--------------------------------------|---|-------------------------------------|--------------|
| 4.4 | | ENHOUSE GAS EMISSIONS. uld the Project: | | | | |
| | a. | Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment? | | | • | |
| | b. | Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases? | | | • | |

Since the adoption of AB 32, there has been little regulatory guidance regarding quantification of potential greenhouse gas (GHG) impacts. Given the complexity of the overall interactions between various global and regional scale air emissions, it is difficult to determine whether any proposed project would alter any existing conditions. No statewide significance threshold has been adopted. Although the San Joaquin Valley Air Pollution Control District has adopted interim guidance on GHG analysis, this guidance only applies to stationary sources.

The recently revised CEQA Guidelines indicate that the lead agency should use careful judgment in assessing potential GHG impacts. Pursuant to the Guidelines, the lead agency should make a good faith effort to describe a project's potential GHG emissions. The lead agency may, in its discretion, rely on a quantitative or qualitative analysis for these purposes (CEQA Guidelines, Section 15064.4(a))

(a) California is a substantial contributor of global greenhouse gases (GHG's), emitting over 400 million tons of CO₂ a year. Climate studies indicate that California is likely to see an increase of three to four degrees Fahrenheit over the next century. Methane is also an important GHG that potentially contributes to global climate change. GHG's are global in their effect, which is to increase the earth's ability to absorb heat in the atmosphere. As primary GHG's have a long lifetime in the atmosphere, accumulate over time, and are generally well-mixed, their impact on the atmosphere is mostly independent of the point of emission.

The impact of anthropogenic activities on global climate change is apparent in the observational record. Air trapped by ice has been extracted from core samples taken from polar ice sheets to determine the global atmospheric variation of CO₂, methane, and nitrous oxide from before the start of the industrialization (approximately 1750), to over 650,000 years ago. For that period, it was found that CO₂ concentrations ranged from 180 parts per million (ppm) to 300 ppm. For the period from approximately 1750 to the present, global CO₂ concentrations increased from a pre-industrialization period concentration of 280 ppm to 379 ppm in 2005, with the 2005 value far exceeding the upper end of the pre-industrial period range.

The Intergovernmental Panel on Climate Change (IPCC) constructed several emission trajectories of GHG's needed to stabilize global temperatures and climate change impacts. It concluded that a stabilization of GHG's at 400 to 450 ppm carbon dioxide-equivalent concentration is required to keep mean global climate change below 2"C, which in turn is assumed to be necessary to avoid dangerous climate change.

City of Lodi Greenhouse Gas Emissions

In accordance with Assembly Bill 32 (AB 32) 2006 and Senate Bill (SB 97) 2007, the City of Lodi is implementing a policy that requires Negative Declarations, Mitigated Negative Declarations and Environmental Impact Reports prepared to comply with CEQA to include a GHG Emissions analysis. The adverse impacts of global climate change include impacts to water supply, air quality, fire hazards, sea level rise (flooding), and an increase in health related problems. AB 32 establishes a state goal of reducing GHG emissions to 1990 level by the year 2020. The long range reduction goal is reflected in Executive Order S-3-05, which requires GHG to be reduced to 80 percent below 1990 levels by 2050.

When dealing with air quality issues related to operation emissions, thresholds are usually compared to the net change in emissions compared to baseline conditions (normally existing conditions with no Project). In addition, there are currently no health-based standards that measure the threat GHGs, including CO₂, pose on human health.

In comparison to existing conditions, implementation of the proposed Harney Lane Specific plan would not increase vehicle emissions generated by mobile source as well as emissions generated by stationary sources because it does not propose physical improvements or construction activities. The City's General Plan is consistent with the State's goal of reducing GHG emissions to 1990 levels by 2020 and is consistent with the City of Lodi General Plan 2010 and accompanying EIR. Therefore, no impact is anticipated.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(b) As stated previously, implementation of the proposed Project would not conflict with applicable regional or local plans, policies or regulations adopted for the purpose of reducing the emissions of greenhouse gases. The proposed Project would be consistent with the State's goals of reducing GHG emissions to 1990 levels by 2020. As such, the proposed Project's contribution to climate change/worldwide GHG emissions would be less than significant.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

| Issues | | | Potentially Significant Impact | Potentially Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|----|---|--------------------------------------|---|-------------------------------------|--------------|
| 4.5 | | BIOLOGICAL RESOURCES Would the proposal: | | | | |
| | a. | Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? | | • | | |
| | b. | Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? | | | | • |
| | C. | Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means? | | | | • |
| | d. | Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of wildlife nursery sites? | | | | • |
| | e. | Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance? | | | | • |
| | f. | Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Conservation Community Plan, or other approved local, regional, or state habitat conservation plan? | | | | • |

(a) No impacts to biological resources are expected as a result of the Harney Lane Specific Plan. All future constructions plans would by reviewed for environmental impact on project-by-project basis. Further, the Project area is within and consistent with the San Joaquin County Multi-Species Habitat Conservation and Open Space Plan (SJMSCP), as amended, as reflected in the conditions of project approval for this proposal. Pursuant to the Final EIR/EIS for the San Joaquin county Multi-Species Habitat Conservation and Open Space Plan (SJMSCP), dated November 15, 2000, and certified by the San Joaquin Council of Governments on December 7, 2000, implementation of the SJMSCP is expected to reduce impacts to biological resources resulting from the proposed project to

a level of less-than-significant. That document is hereby incorporated by reference and is available for review during regular business hours at the San Joaquin Council of Governments (555 East Weber Avenue/Stockton, CA 95202) or online at www.sjcog.org.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(b) The proposed Harney Lane Specific Plan does not involve construction activities. Impacts to Biological Resources have been exhaustively examined and mitigation measures have been detailed in the City's General Plan EIR (SCH #2009022075) and mitigation polices are incorporated in the General Plan policy. All future projects and developments in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis. Therefore, no impact is anticipated.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(c) A significant impact may occur if wetlands that are protected under federal regulation, as defined by Section 404 of the Clean Water Act, would be modified or removed. No construction activities have been proposed as part of the Harney Lane Specific Plan.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(d) A significant impact may occur if the proposed Project interferes or removes access to a migratory wildlife corridor or impedes the use of native wildlife nursery sites. The area north of the Project site lies within the City of Lodi and is currently developed. The area east, south and west is currently agricultural fields. Given the existing development north of the site and regular disturbance associated with agricultural uses, it is unlikely that the site would serve as a migratory corridor or a nursery site. Furthermore, the project area where the Harney Lane Specific Plan would be implemented is not identified as a missing linkage on the California Wilderness Coalition California's Missing Linkages Report. Therefore, no impact is anticipated due to the implementation of the proposed Harney Lane Specific Plan.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(e) A significant impact may occur if the proposed Project would cause an impact that was inconsistent with local regulations pertaining to biological resources, including protected trees. There are no locally designated natural communities within or adjacent to the project area, and the proposed project would not result in the removal of any

heritage trees. Further, the City of Lodi General Plan (Conservation Element) includes goals and policies intended to protect sensitive native vegetation and wildlife habitats. The proposed project would not result in the removal of any heritage trees. Thus, no impact would result.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(f) A significant impact may occur if the proposed Project were inconsistent with mapping or policies in any conservation plans of the types cited. In an effort to protect sensitive and threatened species throughout San Joaquin County, SJCOG prepared the SJMSCP. The purpose of the SJMSCP is to provide for the long-term management of plant, fish and wildlife species, specially those that are currently listed or may be listed in the future under the FESA or CESA, and to provide and maintain multiple-use open space that contributes to the quality of life of residents of San Joaquin County. The City of Lodi has adopted the SJMSCP and participation by the Project in the plan is required by the City.

The proposed project is consistent with the San Joaquin County Multi-Species Habitat Conservation and Open Space Plan (SJMSCP), as amended, as reflected in the conditions of project approval for this proposal. Pursuant to the Final EIR/EIS for the San Joaquin County Multi-Species Habitat Conservation and Open Space Plan (SJMSCP), dated November 15, 2000, and certified by the San Joaquin Council of Governments on December 7, 2000, implementation of the SJMSCP is expected to reduce impacts to biological resources resulting from the proposed project to a level of less-than-significant. That document is hereby incorporated by reference and is available for review during regular business hours at the San Joaquin Council of Governments (555 E. Weber Avenue, Stockton, CA 95202) or online at: www.sicoq.orq.

Significance Determination: No Impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|----|--|--------------------------------------|---|-------------------------------------|--------------|
| 4.6 | | ULTURAL RESOURCES uld the Project: | | | | |
| | a. | Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5? | | | • | |
| | b. | Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5? | | | • | |
| | c. | Directly or indirectly destroy a unique paleontological resource or unique geologic feature? | | | • | |
| | d. | Disturb any human remains, including those interred outside of formal cemeteries. | | | • | |

(a) A significant impact would occur if the Project caused a substantial adverse change to a historical resource through demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of the historical resource would be materially impaired. The proposed Harney Lane Specific Plan does not include construction, grading, and site disturbance. Therefore, the Project would have less than significant impact on historical resources as defined by CEQA

Significance Determination: Less than significant.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(b) A significant impact would occur if the Project caused a substantial adverse change to a historical resource through demolition, destruction, relocation, or alteration of the resource or its immediate surroundings such that the significance of the historical resource would be materially impaired. The proposed Harney Lane Specific Plan does not involve construction, grading, and site disturbance. All future construction activities would be evaluated for adverse environmental impact on project-by-project basis. Therefore, the Project would have less than significant impact on historical resources as defined by CEQA

Significance Determination: Less than significant.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(c) A significant impact may occur if grading or excavation activities associated with the proposed Project would disturb paleontological resources or geologic features that exist within the Project site. No paleontological resources or unique geologic features have been noted on the surface of the Project site. The likelihood of paleontological resources or unique geologic features being present subsurface within the boundaries of the

proposed Project is unlikely given the rapid rate of deposition in the area. The possibility exists, however, that previously unidentified paleontological resources could be encountered during ground-disturbing activities. All future ground disturbing activities would be evaluated on project-by-project basis for environmental impacts. Therefore, implementation of the proposed Harney Lane Specific Plan would lead to less than significant impact.

Significance Determination: Less than significant.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(d) A significant impact may occur if grading or excavation activities associated with the proposed Project would disturb previously interred human remains. Disturbing human remains, either in a formal cemetery or disarticulated, would be considered a significant impact under CEQA Guidelines §10564.5. The proposed Harney Lane Specific Plan does not include construction, grading, and site disturbance. Therefore, the Project would have less than significant impact on historical resources as defined by CEQA.

Significance Determination: Less than significant.

Mitigation Measures: Mitigation measures are not required

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|-----------|---------------------|--|--|--|-------------------------------------|---------------|
| 4.7 | | OLOGY AND SOILS. eld the Project: | | | | |
| | a. | Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving: | | | | |
| | | i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42. | | | | |
| | | ii. Strong seismic ground shaking? | | | | |
| | | iii. Seismic-related ground failure, including liquefaction? | | | | • |
| | | iv. Landslides? | | | | |
| | b. | Result in substantial soil erosion, or the loss of topsoil? | | | | • |
| | C. | Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the Project, and potentially result in on-or off-site landslide, lateral spreading, subsidence, liquefaction or collapse? | | | | • |
| | d. | Be located on expansive soils, as defined in Table 18-1-13 of the Uniform Building Code (1994), creating substantial risks to life or property? | | | | • |
| | e. | Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater? | | | | • |
| (a) i. | to infinite oth exp | significant impact may occur if the proposed adverse effects involving fault rupture, sucrastructure within a state-designated Alquier designated fault zone. The proposed Hoose people or structures to potential substanture as the Project involves no construction | ch as from uist-Priolo Iarney Lar ntial adven | placement Earthquake ne Specific rse effects in | of structu Fault Zo Plan wou | one or ld not |

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

ii. A significant impact may occur if the proposed Project results in or exposes people to adverse effects involving strong ground shaking from fault rupture or seismic hazards. There is no record of any seismic activity originating in the City of Lodi other than tremors on the west side of the San Joaquin Valley, close to the Ortigalita Fault. The proposed Harney Lane Specific Plan would not expose people or structures to potential substantial adverse effects involving surface rupture as the Project involves no construction activities. No impact.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact.

iii. A significant impact may occur if the Project were to result in or expose people to adverse effects involving seismic-related ground failure from liquefaction and other geologic hazards. Liquefaction is a form of earthquake-induced ground failure that occurs primarily in relatively shallow, loose, granular, water-saturated soils. The potential for liquefaction is recognized throughout the San Joaquin Valley where unconsolidated sediments and a high water table coincide. Areas which have the greatest potential for liquefaction are those areas in which the water table is less than 50 feet below the ground surface and soils are predominantly clean, comprised of relatively uniform sands and are of loose to medium density. However, the proposed Harney Lane Specific Plan would not expose people or structures to potential substantial adverse effects involving surface rupture as the Project involves no construction activities. No impact.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact.

iv. A significant impact may occur if the Project results in or exposes people to adverse effects involving landslides. Slope stability hazards are nonexistent and present no risk in the City of Lodi. The Project site is located in an area of generally level terrain that would not produce a landslide. Average grade within the Project site is between zero and five degrees. Further, according to the Official Maps of Seismic Hazard Zones provided by the State of California Department of Conservation, the Project site is not located within an earthquake-induced landslide zone, which is defined as an area where previous occurrence of landslide movement, or local topographic, geological, geotechnical and subsurface water conditions indicate a potential for permanent ground displacement.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact.

(b) The project site would be subject to seismic ground shaking, as is the case throughout seismically active California. Ground shaking may occur as result of movement along

any fault in northern California. However, the proposed Harney Lane Specific Plan does not involve construction activities or improvements.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(c) The project is an implementing arm of the City General Plan and does not propose any physical improvements or construction activities. Therefore, no impact would occur.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(d) See discussion under a) above. Potential impacts are highly unlikely and are considered to be less than significant and no mitigation is required.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(e) No septic tanks or alternative wastewater disposal systems are necessary to support the Proposed Project. Therefore, no impact would occur and no mitigation is required.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|---|---|--------------------------------------|---|-------------------------------------|--------------|
| 4.8 | HAZARDS AND HAZARDOUS MATERIALS. Would the Project: | | | | | |
| | a. | Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials? | | | | • |
| | b. | Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment? | | | | • |
| | c. | Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school? | | | | • |
| | d. | Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment? | | | | • |
| | e. | For a Project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the Project result in a safety hazard for people residing or working in the Project area? | | | | • |
| | f. | For a Project within the vicinity of a private airstrip, would the Project result in a safety hazard for people residing or working in the Project area? | | | | • |
| | g. | Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan? | | | | • |
| | h. | Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands? | | | | • |

(a) A significant impact may occur if the proposed project involves the use or disposal of hazardous materials as part of its routine operations and has the potential to generate toxic or otherwise hazardous emissions. The proposed project involves adoption of a street widening plan and not the routine transport, use, or disposal of hazardous materials or the generation of toxic or hazardous emissions. In addition, the project involves no construction activities.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(b) A significant impact may occur if the proposed project uses substantial amounts of hazardous materials as part of routine operations, which could pose a hazard under accident or upset conditions. The operation of the roadway does not involve the use of hazardous materials. Vehicles carrying hazardous materials may use the roadway. Nonetheless, the proposed project would not increase the potential for accidents or spills beyond existing conditions. Furthermore, improvements in traffic flow may reduce the potential for accidents overall; therefore, no impacts would occur.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(c) A significant impact may occur if the proposed project is located within 0.25 mile of an existing or proposed school site and projected to release toxic emissions that pose a hazard beyond regulatory thresholds. There several private and public schools within a ½ mile of the project area. However, the project does not involve construction activities and does not involve. The use of hazardous materials or result in the release of hazardous materials or substances.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(d) A significant impact may occur if the proposed project site contains hazardous materials that would create a significant hazard to the public or the environment. California Government Code Section 65962.5 requires state agencies to compile lists of hazardous waste disposal facilities, unauthorized releases from underground storage tanks, contaminated drinking water wells, and solid waste facilities from which there is known hazardous waste and submit such information to the Secretary for Environmental Protection on at least an annual basis.

There are two sites identified as hazardous material sites within the project area. However, the Harney Lane Specific Plan does not involve construction activities. All future construction activities would be subject to standard City procedures and other applicable State and Federal procedures and requirements.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(e) A significant impact may occur if the proposed Project site is located within a public airport land use plan area or within 2 miles of a public airport and would create a safety hazard.

The Project site is located with the area of influence for the Lodi Airpark and Kingdon Executive Airport. The Lodi Airpark is located roughly 4 miles to the southwest of the Project site while the Kingdon Executive Airport is located approximately 4 miles southwest of the Project site. The primary function of the Lodi Airpark is as a base for a commercial aerial chemical application service for both agriculture and insect abatement purposes. The Lodi Airpark is also used for pilot training activity. The Kingdon Executive Airport presently hosts a variety of aviation activities including pilot training and aerial application of agricultural chemicals. The airport is also home to the Delta Flying Club, which owns six single-engine piston aircraft for use by its members.

The Project site is located outside of the Part 77 Horizontal Surface zone of both airports, which consists of the airport's primary, horizontal, conical, approach and transitional surfaces. Therefore, impacts related to safety hazards for people visiting or working within the Project site would be less than significant.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(f) A significant impact may occur if the proposed Project is located within the vicinity of a private airstrip and creates a safety hazard for people in the Project area. The Project site is located outside of the Part 77 Horizontal Surface zone of both airports, which consists of the airport's primary, horizontal, conical, approach and transitional surfaces. Therefore, impacts related to safety hazards for people visiting or working within the Project site would be less than significant.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(g) A significant impact may occur if the proposed Project were to interfere with roadway operations occurring in conjunction with an emergency response plan or emergency evacuation plan or generate enough traffic to create traffic congestion that would interfere with the execution of such a plan.

The Project would not impair implementation of or physically interfere with an adopted emergency response or evacuation plan. All construction-related activities would be contained within and immediately around the Project site. Road closures are not anticipated during construction activities; however, in the event that a closure is

necessary standard contractor specifications imposed by the City include a requirement to ensure that roadways surrounding the Project site remain accessible to emergency vehicles and crews, and open for emergency evacuations, if necessary. The City has an Emergency Management Plan that addresses the campus community's planned response for various levels of emergencies, including fires, hazardous spills, earthquakes, flooding, and explosions

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

A significant impact may occur if the proposed Project is located in or adjacent to a wildland area and places persons or structures at risk in the event of a fire. The City's newly adopted General Plan (2010) identifies both urban and wildland fire hazards exist in the Lodi Planning Area, creating the potential for injury, loss of life, and property damage. Urban fires primarily involve the uncontrolled burning of residential, commercial, and/or industrial structures due to human activities. Factors that exacerbate urban structural fires include substandard building construction, highly flammable materials, delayed response times, and inadequate fire protection services.

The City of Lodi is not characterized by substantial areas of wildlands. The topography of the City is relatively homogenous and steep slopes that could contribute to wildland fires are not common. The City's General Plan indicates that less than one percent of the City and its immediate vicinity has "Moderate" fire hazard potential. In the event of a fire, the Fire Department relies on sufficient water supply and pressure. The City's design standard for water transmission facilities is to provide 4,000 gallons per minute of flow at a minimum 45 pounds per square inch of pressure in pipes 8 inches and larger. The Project area is made up of Non-Wildland/Non-Urban zones, Urban/Unzoned, and Moderate Risk zones. Therefore, the proposed Project would not expose people or structures to a significant risk of loss, injury, or death involving wildland fires, including where wildland fires are adjacent to urbanized areas. As such, there would be no impact.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

| Issue | s | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|-------|----|--|--------------------------------------|---|-------------------------------------|--------------|
| 4.9 | | ROLOGY AND WATER QUALITY uld the Project: | | | | |
| | a. | Violate any water quality standards or waste discharge requirements? | | | • | |
| | b. | Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (i.e., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? | | | | |
| | C. | Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site? | | | • | |
| | d. | Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site? | | | • | |
| | e. | Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff? | | | • | |
| | f. | Otherwise substantially degrade water quality? | | | • | |
| | g. | Place housing within a 100-year flood hazard area, as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? | | | • | |
| | h. | Place within a 100-year floodplain structures which would impede or redirect flood flows? | | | • | |
| | i. | Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam? | | | | • |
| | j. | Inundation by seiche, tsunami, or mudflow? | | | | |

(a) A significant impact may occur if the proposed project discharges water that does not meet the water quality standards set by agencies that regulate surface water quality and water discharge into stormwater drainage systems.

The Harney Lane Specific Plan does not involve physical improvements or construction activities. Future construction activities and projects would be reviewed project-by-project basis. Although implementation of the proposed Project would increase impermeable surface area, and site runoff, potentially contributing typical roadway pollutants to the environment, future developments within the Project area would be required to conform to surface water quality standards adopted by the Regional Water Quality Control Board and enforced by the City of Lodi. These standards mandate installation of either biological or mechanical methods of treating and cleansing stormwater runoff prior to entering the City and regional drainage system, or equivalent water quality features. With adherence to these requirements, this impact would be less-than-significant.

Significance Determination: Less than significant

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(b) Groundwater is a major component of the water supply for many public water suppliers in the Valley. It is also used by private industry, as well as by private agricultural and domestic users. A project would normally have a significant impact on groundwater supplies if it were to result in a demonstrable and sustained reduction in groundwater recharge capacity or change the potable water levels enough to reduce the ability of a water utility to use the groundwater basin for public water supplies or the storage of imported water, reduce the yields of adjacent wells or well fields, or adversely change the rate or direction of groundwater flow.

The Harney Lane Specific Plan does not involve construction activities. All future construction activities would be subjected to environmental review on project-by-project basis.

Significance Determination: Less than significant

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(c) A significant impact may occur if the proposed project results in a substantial alteration of drainage patterns and a substantial increase in erosion or siltation during construction or operation of the project.

The Project site does not contain any discernable watercourses, topographical depressions, or bodies of standing water. No streams or river courses are located on or immediately adjacent to the project site. As such, no impact would occur that would affect a nearby stream or river or the existing drainage pattern on or near the proposed project site.

Significance Determination: Less than significant

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(d) Refer to c), above. The proposed project would not substantially alter the existing drainage pattern of the site or area. A significant impact may occur if the proposed project results in increased runoff volumes during construction or operation and flooding conditions that affect the project site or nearby properties. The Harney Lane Lane Specific Plan does not involve construction activities.

Significance Determination: Less than significant

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(e) The Harney Lane Specific Plan involves adoption of a road widening policy. Implementation of the Harney Lane Specific Plan would result in conditions similar to existing, and would not propose any new uses that would potentially degrade water quality. All future construction activates would be subject to environmental review on project-by-project basis.

Significance Determination: Less than significant

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(f) A significant impact may occur if a project includes potential sources of water pollutants with the potential to substantially degrade water quality.

The proposed Harney Lane Specific Plan would neither create nor contribute to water quality degradation. Future construction activities would be required to comply with City of Lodi and Regional Water Quality Control Board surface water quality standards, including applicable NPDES requirements, which require contractors to take measures to prevent the pollution of channels, storm drains, and bodies of water during construction. As such, implementation of the proposed project would not create any new impacts related to water quality beyond those that already exist. Therefore, no impact related to water quality would occur.

Significance Determination: Less than significant

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(g) A significant impact may occur if the proposed project is located within a 100-year flood zone. The proposed Project would not place housing within a 100-year flood hazard area identified on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map because the Project does not include a residential component that would be affected by flooding potential, so no impact would occur.

Significance Determination: Less than significant

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(h) A significant impact may occur if the proposed project is located within a 100-year flood zone and would impede or redirect flood flows.

As discussed in Checklist Response 3.9 (G) above, the project site is not located within a 100-year flood hazard area. In addition, the proposed project would not include the construction of any structures. Therefore, no impact would occur.

Significance Determination: Less than significant

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(i) A significant impact may occur if the proposed Project is located in a flood-prone area, including floods caused by the failure of a dam or levee.

The Project sites, as well as the entire City of Lodi, are located in a dam inundation area for the Pardee and Camanche Dam and dike system. Flood water from the Pardee dam would take 4 hours and 20 minutes to reach west Lodi, and flood water from the Camanche Dam and dike system would take 4 to 6 hours to reach Lodi. Due to the location of the proposed Project, the impacts associated with seiches, tsunami, and extreme high tides or sea level change would be considered low.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(j) A significant impact may occur if the proposed project is located in an area with inundation potential due to seiche, tsunami, or mudflow.

A seiche is the tide-like rise and drop of water in a closed body of water caused by earthquake-induced seismic shaking or strong winds. A tsunami is a series of large waves generated by a strong offshore earthquake or volcanic eruption. Given the substantial distance of the Project site from San Francisco Bay or the Pacific Ocean, tsunami waves would not be a threat to the site. There is no large body of water on or within the vicinity of the Project site. The subject area is flat and does not have any steep slopes or hillsides that would be susceptible to mudflows or landslides. Therefore, no impact would occur.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|--|--|--------------------------------------|---|-------------------------------------|--------------|
| 4.10 | LAND USE AND PLANNING. Would the Project: | | | | | |
| | a. | Physically divide an established community? | | | | • |
| | b. | Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the Project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating on environmental effect? | | | | • |
| | c. | Conflict with any applicable habitat conservation plan or natural community conservation plan? | | | | • |

(a) A significant impact may occur if the proposed project is sufficiently large enough or otherwise configured in such a way so as to create a physical barrier within an established community.

The Harney Lane Specific Plan involves road a widening program and involves no construction activities. The Harney Lane Specific Plan would improve east-west mobility in the southern part of the City. As such, it would not create a physical barrier within an established community.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(b) A significant impact may occur if the proposed project is inconsistent with general plan designations or zoning currently applicable to the proposed project site and causes adverse environmental effects, which the general plan and zoning ordinance are designed to avoid or mitigate.

The purpose of this Harney Lane Specific Plan is to reach a consensus of the most appropriate roadway improvements to be implemented in the Harney Lane corridor to accommodate anticipated growth and traffic volume increases and establish the necessary right of way needed to be acquired, reserved and/or dedicated in order to accommodate the roadway improvements. The Specific Plan was prepared in accordance with the City framework set out in the General Plan adopted April 7, 2010. Items considered during the preparation of the Specific Plan include future land use, roadway and bicycle network, right of way issues, safety (pedestrian and vehicular), ingress and egress for residents and businesses and environmental issues. There are no physical improvements or construction activities proposed by the project itself at this time. Subsequent development in the Plan Area, including all Subdivisions, Site Plan

Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(c) A significant impact may occur if the proposed project conflicts with a habitat conservation plan or natural community conservation plan adopted for the area surrounding the project location.

As discussed in 3.10 (B) above, there are no physical improvements or construction activities proposed by the Harney Lane Specific Plan. All future developments in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--|----|--|--------------------------------------|---|-------------------------------------|--------------|
| 4.11 MINERAL RESOURCES Would the Project: | | | | | | |
| | a. | Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the State? | | | | • |
| | b. | Result in the loss of availability of a locally- important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? | | | | • |

(a) A significant impact may occur if the proposed project is located in an area that is used or available for extraction of a regionally important mineral resource, converts an existing or potential regionally important mineral extraction use to another use, or affects access to a site used or potentially available for regionally important mineral resource extraction.

The Harney Lane Specific Plan involves a road widening program and no physical improvements or construction activities proposed by the Harney Lane Specific Plan. All future developments in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(b) A significant impact may occur if a project is located in an area that is used or available for extraction of a locally important mineral resource, as delineated on a local general plan, specific plan, or other land use plan.

As discussed in 3.11(A), no physical improvements or construction activities are proposed by the project itself at this time. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

Significance Determination: No Impact.

Mitigation Measures: Mitigation measures are not required

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|------|---|--------------------------------------|---|-------------------------------------|--------------|
| 4. 12 | NOI: | SE uld the Project result in: | | | | |
| | a. | Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies? | | | • | |
| | b. | Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels? | | | • | |
| | c. | A substantial permanent increase in ambient noise levels in the Project vicinity above levels existing without the Project? | | | • | |
| | d. | A substantial temporary or periodic increase in ambient noise levels in the Project vicinity above levels existing without the Project? | | | • | |
| | e. | For a Project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the Project expose people residing or working in the Project area to excessive noise levels? | | | • | |
| | f. | For a Project within the vicinity of a private airstrip, would the Project expose people residing or working in the Project area to excessive noise levels? | | | • | |

(a) A significant impact may occur if the proposed project generates noise levels that exceed the standards for ambient noise, as established by the general plan and municipal code, and/or exposes persons or sensitive uses to increased noise levels. Noise-sensitive uses may include residences, transient lodging, schools, libraries, churches, hospitals, nursing homes, auditoriums, concert halls, amphitheatres, playgrounds, and parks.

The Harney Lane Specific Plan involves a road widening program and involves no physical improvements or construction activities. All future developments within the project limits will be subject to environmental review on a project-by-project basis.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(b) A significant impact may occur if the project results in or exposes people to excessive groundborne vibration or groundborne noise levels during construction or operation. This would include excessive groundborne vibration or noise that causes structural damage or displaces objects in nearby buildings.

As discussed in 2.12(A), no physical improvements or construction activities are proposed by the project itself at this time. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(c) A significant impact may occur if the proposed project were to result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the proposed project.

The Harney Lane Specific Plan involves a road widening program and no physical improvements or construction activities proposed are by the Harney Lane Specific Plan. All future developments in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis. In addition, noise levels in the project vicinity are dominated by vehicular traffic along Harney Lane and the nearby trains. This condition would continue after implementation of the proposed project. The proposed project is intended to reduce congestion. The Harney Lane Specific Plan is not growth-inducing. The resultant increase in traffic noise is estimated to be of a level that would not be readily noticeable to the typical human ear in the community environment (i.e., outside of controlled conditions). Therefore, the increase in traffic noise would be less than significant.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(d) A significant impact may occur if the proposed project were to result in a substantial temporary or periodic increase in ambient noise levels above existing ambient noise levels without the proposed project.

As discussed in 3.12(C), no physical improvements or construction activities proposed by the project itself at this time. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis. As such, implementation of the Harney Lane Specific Plan would not substantially increase ambient noise levels in the project vicinity over existing conditions.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

(e) A significant impact may occur if the project is located within an airport land use plan or within 2 miles of a public airport and people residing or working in the project area would be exposed to excessive noise levels.

The proposed Harney Lane Specific Plan would not expose people residing or working in the Project area to excessive noise levels generated by public use airports, or private airstrips. There is not an airport located within two (2) miles of the Project site. The closest airport to the Project site is the Lodi Airpark, located approximately four (4) miles southwest of the Project site, and supports twenty to thirty (20-30) operations per day. The airport's noise "footprint" does not extend beyond the immediate airport boundary. Therefore, the Project would have no impact from airport-generated noise.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(f) A significant impact may occur if the project is located in the vicinity of a private airstrip and people residing or working in the project area would be exposed to excessive noise levels.

The proposed Harney Lane Specific Plan would not expose people residing or working in the Project area to excessive noise levels generated by public use airports, or private airstrips. There is not an airport located within two (2) miles of the Project site. The closest airport to the Project site is the Lodi Airpark, located approximately four (4) miles southwest of the Project site, and supports twenty to thirty (20-30) operations per day. The airport's noise "footprint" does not extend beyond the immediate airport boundary. Therefore, the Project would have no impact from airport-generated noise.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|----|---|--------------------------------------|---|-------------------------------------|--------------|
| 4.13 | | PULATION AND HOUSING uld the Project: | | | | |
| | a. | Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (e.g., through extension of roads or other infrastructure)? | | | • | |
| | b. | Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere? | | | | • |
| | c. | Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere? | | | | • |

(a) A significant impact may occur if the proposed project induces substantial population growth in an area, either directly or indirectly.

The proposed Harney Lane Specific Plan does not involve the development of housing. The proposed project is General Plan policy program designed to mitigate anticipated traffic conditions. It would not induce population growth directly or indirectly. Therefore, no impact would occur.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(b) A significant impact may occur if the proposed project would result in the displacement of existing housing units, necessitating construction of replacement housing elsewhere.

The proposed Harney Lane Specific Plan would not induce population growth in the area either directly or indirectly. The proposed Harney Lane Specific Plan involves infrastructure improvements along Harney Lane and would not displace existing housing in the area. No replacement housing would be required as a result of the proposed Harney Lane Specific Plan. No impacts would occur in this regard.

Significance Determination: No impact

Mitigation Measures: Mitigation measures are not required

(c) A significant impact may occur if the proposed project results in the displacement of a substantial number of people.

Please refer to 3.13(B). The proposed Harney Lane Specific Plan would not displace residents, and, therefore, no replacement housing would be required. As such, no impacts would occur.

Significance Determination: No impact

Mitigation Measures: Mitigation measures are not required

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|--|--|--------------------------------------|---|-------------------------------------|--------------|
| 4.14 | Woi impo alter alter caus accep | LIC SERVICES uld the Project result in substantial adverse physical acts associated with the provision of new or physically red governmental facilities, need for new or physically red governmental facilities, the construction of which could be significant environmental impacts, in order to maintain be table service ratios, response times or other performance ctives for any of the public services: | | | | |
| | a. | Fire protection? | | | | |
| | b. | Police protection? | | | | |
| | c. | Schools? | | | • | |
| | d. | Parks? | | | | |
| | e. | Other public facilities? | | | • | |

(a) A significant impact may occur if the City of Lodi Fire Department cannot adequately serve the proposed project based on response time, access, or fire hydrant/water availability.

The City of Lodi Fire Department provides fire protection and emergency response to the City. The Lodi Fire Department operates out of four stations. The proposed Harney Lane Specific Plan would occur within and along Harney Lane. The Harney Lane Specific Plan would not generate new residents or employees, and would not result in a demand of fire and emergency response services. Future construction activities would be reviewed on project-by-project basis to ensure compliance and consistency with the City's Safety policy. Implementation of the proposed Harney Lane Specific Plan would improve traffic flow and emergency access within the project area. Therefore, impacts are less than significant.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(b) A significant impact may occur if the proposed project results in an increase in demand for police services that would exceed the capacity of the police department responsible for serving the site.

The City of Lodi Police Department provides police protection to the City. The main police station is located at 215 West Elm Street, approximately 3.25 miles north of the project site. The proposed Harney Lane Specific Plan does not include uses that would require additional police services or facilities. Long-term project operations would improve traffic flow and thus police access within the project area. Therefore, impacts are less than significant.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(c) A significant impact may occur if the proposed project induces substantial employment or population growth, which could generate demand for school facilities that exceed the capacity of the school district responsible for serving the project site.

The proposed Harney Lane Specific Program is a street-widening project intended to relieve existing and future traffic congestion. It would not induce growth, either directly or indirectly, and would not increase the demand for schools in the area through substantial employment or population growth. No impacts are anticipated related to population or employment growth; therefore, no impacts on enrollment levels at nearby schools would occur.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(d) A significant impact may occur if the available parks and recreation services cannot accommodate the population increase resulting from implementation of the proposed project.

The proposed Harney Lane Specific Program is a street-widening project intended to relieve existing and future traffic congestion. It would not induce growth, either directly or indirectly. Therefore, it would not increase the demand for parks in the area.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required **Significance After Mitigation:** No impact

(e) A significant impact may occur if the proposed project generates demand for other public facilities, thereby exceeding the capacity available to serve the project site.

The Project would not contribute significantly to the demand for any other public facilities (e.g., library, senior centers, or other public facilities/services) as it would not directly introduce a new population of residents to the City. Some minor incidental demand for services may result, as such impacts would be less than significant on a Project-specific or cumulative basis.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

| | Issues | | | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|------|-----------------|---|--|---|-------------------------------------|--------------|
| 4.15 | 4.15 RECREATION | | | | | |
| | a. | Would the Project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? | | | • | |
| | b. | Does the Project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment? | | | • | |

(a) A significant impact may occur if the proposed project includes substantial employment or population growth, which could generate demands for public parks and recreational facilities that exceed the capacity of those that currently exist.

As discussed previously, the proposed Harney Lane Specific Plan would not directly or indirectly induce growth. Therefore, the proposed project would not increase the use of existing neighborhood and regional parks or any other recreation facilities. The proposed project would have no impact on neighborhood or regional parks.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(b) A significant impact may occur if the proposed project includes the construction or expansion of recreational facilities or necessitates the construction or expansion of recreational facilities that might have an adverse physical effect on the environment.

The proposed Harney Lane Specific Plan does not include recreational component. The proposed project would not require the construction or expansion of recreational facilities or induce growth, either directly or indirectly. Therefore, it would not increase the demand for recreational facilities in the area.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|----|---|--------------------------------------|---|-------------------------------------|--------------|
| 4.16 | | NSPORTATION/TRAFFIC ald the Project: | | | | |
| | a. | Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)? | | | • | |
| | b. | Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated roads or highways? | | | • | |
| | c. | Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks? | | | • | |
| | d. | Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? | | | • | |
| | e. | Result in inadequate emergency access? | | | | |
| | f. | Result in inadequate parking capacity? | | | | |
| | g. | Conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)? | | | | • |

(a) A significant impact may occur if the proposed project causes an increase in traffic that is substantial in relation to the existing traffic load and capacity of the street system.

The proposed Harney Lane Specific Plan would widen Harney Lane between State Highway 99 and Lower Sacramento Road in order to reduce congestion and increase sidewalk widths to improve pedestrian access. No additional vehicle trips would be generated by the proposed project. The project is designed to ease existing congestion in the area and to provide additional capacity for the future developments. No major shift in traffic is expected as a result of the street improvements. Therefore, no impact would occur.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

(b) A significant impact may occur if the proposed project exceeds, either individually or cumulatively, a level of service standard established by the San Joaquin Council of Governments, the county congestion management agency, for designated roads or highways.

Please refer to 3.11(A). The purpose of a Congestion Management Program (CMP) is to develop a coordinated approach to managing and decreasing traffic congestion by linking the various transportation, land use, and air quality planning programs throughout the County. The CMP program required review of substantial individual projects, which might individually impact the CMP transportation system. The proposed Harney Lane Specific Plan does not generate any new daily trips. The proposed Harney Lane Specific Plan would widen the existing roadway and add additional travel lanes, which would improve traffic flow. The project aims to reduce congestion and, as such, would help maintain an acceptable level of service (LOS) along the affected portion of Harney Lane. Therefore, less than significant impact would occur.

Significance Determination: Less than significant impact Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(c) A significant impact may occur if the proposed project changes air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks.

There are no airports located within or adjacent to the project limits. The proposed project does not include any aviation-related elements and would not change existing air traffic patterns. Therefore, no impact would occur.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required **Significance After Mitigation**. No impact

Significance After Mitigation: No impact

(d) A significant impact may occur if the proposed project substantially increases road hazards due to a design feature or introduced incompatible uses.

The proposed project would not increase road hazards due to a design feature or introduce incompatible uses. The Harney Lane Specific Plan involves reservation and acquisition of right-of-ways for future road widening and proposes no physical improvements or construction activities. The proposed project would incorporate design features to improve circulation, reduce congestion, and increase safety along Harney Lane.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

(e) A significant impact may occur if the proposed project results in inadequate emergency access.

The proposed project consists of the widening of Harney Lane to improve traffic flow and reduce traffic congestion. These improved conditions could enhance emergency access to the surrounding area. Therefore, no impact related to inadequate emergency access would occur.

Significance Determination: No impact

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(f) A significant impact may occur if the proposed project results in inadequate parking capacity based upon City code requirements.

Construction activities may temporarily reduce available on-street parking in the project area. Impacts on parking during construction would be temporary and, once completed, the project would not result in a net loss of parking, and may even increase parking capacity. Therefore, impacts would be less than significant.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(g) A significant impact may occur if the proposed project conflicts with adopted policies, plans, or programs supporting alternative transportation.

The proposed project would not conflict with adopted policies, plans, or programs supporting alternative transportation. No impact would occur.

Significance Determination: Less than significant impact **Mitigation Measures:** Mitigation measures are not required

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact | | |
|--------|--|--|--------------------------------------|---|-------------------------------------|--------------|--|--|
| 4.17 | 17 UTILITIES AND SERVICE SYSTEMS Would the Project: | | | | | | | |
| | a. | Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board? | | | | - | | |
| | b. | Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? | | | | • | | |
| | c. | Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? | | | | • | | |
| | d. | Have sufficient water supplies available to serve the Project from existing entitlements and resources, or are new or expanded entitlements needed? | | | | • | | |
| | e. | Result in a determination by the wastewater treatment provider which serves or may serve the Project that it has adequate capacity to serve the Project's Projected demand in addition to the provider's existing commitments? | | | | • | | |
| | f. | Be served by a landfill with sufficient permitted capacity to accommodate the Project's solid waste disposal needs? | | | | • | | |
| | g. | Comply with federal, state, and local statutes, and regulations related to solid waste? | | | | • | | |

(a) A significant impact may occur if the proposed project exceeds wastewater treatment requirements of the regional water quality control board, the local regulatory governing agency.

As indicated in the project description, the proposed Harney Lane Specific Plan is an integral part of the City's General Plan 2010 and involves establishment of the necessary right-of-way required to accommodate the roadway improvements. Proposed right-of-way acquisitions would occur in conjunction with actual construction projects in the future. No physical improvements or construction activities are proposed in conjunction with implementation of the Harney Lane Specific Plan. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis. Therefore, no impacts would occur.

Significance Determination: No impact would occur

Mitigation Measures: Mitigation measures are not required

Significance After Mitigation: No impact

(b) A significant impact may occur if the proposed project requires construction of new water or wastewater treatment facilities or expansion of existing facilities.

The project would not require or result in the construction of new water treatment facilities or expansion of existing facilities. In addition, the proposed Harney Lane Specific Plan would not use water in amounts that would have a significant impact on water treatment facilities. The minimal amounts of water used during construction and for irrigation of landscaping would be accommodated by existing water supplies. Therefore, no impact would occur.

Significance Determination: No impact would occur **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(c) A significant impact may occur if the volume of stormwater runoff from the proposed project increases to a level exceeding the capacity of the storm drain system serving the project site.

New storm drainage facilities would be constructed as part of the proposed project as part of future projects. Each project would be reviewed for potential environmental impact on project by project basis. The construction of all storm water drainage facilities required as part of the project would be subject to the requirements of the RWQCB and the NPDES permit process; therefore impacts are considered less than significant. No impact would result due to implementation of the proposed Harney Lane Specific Plan.

Significance Determination: No impact would occur **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(d) A significant impact may occur if the proposed project would exceed the existing water supplies available to serve the project.

The proposed project consists of improvements to an existing street and would not increase the use of water, except for irrigation of landscaping improvements. City policies encourage the use of drought tolerant trees whenever possible to minimize the use of water in the City. The project would not result in new facilities or other uses that would require additional water resources. As a result, existing water supplies would not be exceeded by the project. Therefore, no impact would occur.

Significance Determination: No impact would occur

Mitigation Measures: Mitigation measures are not required

(e) A significant impact may occur if the proposed project would increase wastewater generation to such a degree that the capacity of facilities currently serving the project site would be exceeded.

The proposed project would not create new land uses that would result in wastewater generation that would affect the capacity of existing facilities or wastewater utility infrastructure. Therefore, no impact would occur.

Significance Determination: No impact would occur **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(f) A significant impact may occur if the proposed project were to increase solid waste generation to a degree that existing and projected landfill capacities would be insufficient to accommodate the additional solid waste.

The proposed project would not produce any solid waste during operations. Construction activities may generate minor amounts of solid waste (concrete, asphalt, etc.), but these small amounts would be recycled or disposed of in existing landfills. The amount could be accommodated by existing landfill capacity. Therefore, no impact would occur.

Significance Determination: No impact would occur **Mitigation Measures:** Mitigation measures are not required

Significance After Mitigation: No impact

(g) A significant impact may occur if the proposed project generates solid waste that is not disposed of in accordance with applicable regulations.

Disposal of all solid waste generated would comply with federal, state, and local statutes and regulations related to solid waste. Therefore, no impact would occur.

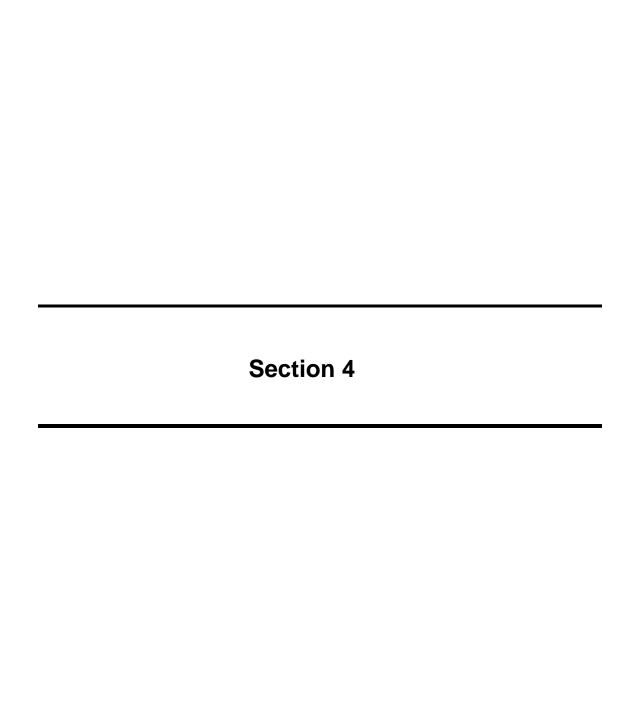
Significance Determination: No impact would occur

Mitigation Measures: Mitigation measures are not required

| Issues | | | Potentially Significant Impact | Less Than Significant With Mitigation Incorporated | Less-Than- Significant Impact | No Impact |
|--------|----|---|--------------------------------------|---|-------------------------------------|--------------|
| 4.18 | | ANDATORY FINDINGS OF GNIFICANCE | | | | |
| | a. | Does the Project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? | | | • | |
| | b. | Does the Project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a Project are considerable when viewed in connection with the effects of past Projects, the effects of other current Projects, and the effects of probable future Projects)? | | | • | |
| | c. | Does the Project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly? | | | • | |

(a) Does the Project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?

Less than Significant impact. As documented in this Initial Study, the proposed Harney Lane Specific Plan does not involve construction activities. The project will not substantially impact any scenic vistas, scenic resources, or the visual character of the area; the proposed project would not directly affect any sensitive habitat or wildlife populations. The project does not involve any operational component or construction impacts that could substantially degrade the quality of the environment, as discussed throughout this analysis. Subsequent development in the Plan Area, including all Subdivisions, Site Plan Reviews, Planned Development Review, and Conditional Use Permits will be subject to environmental review on a project-by-project basis.

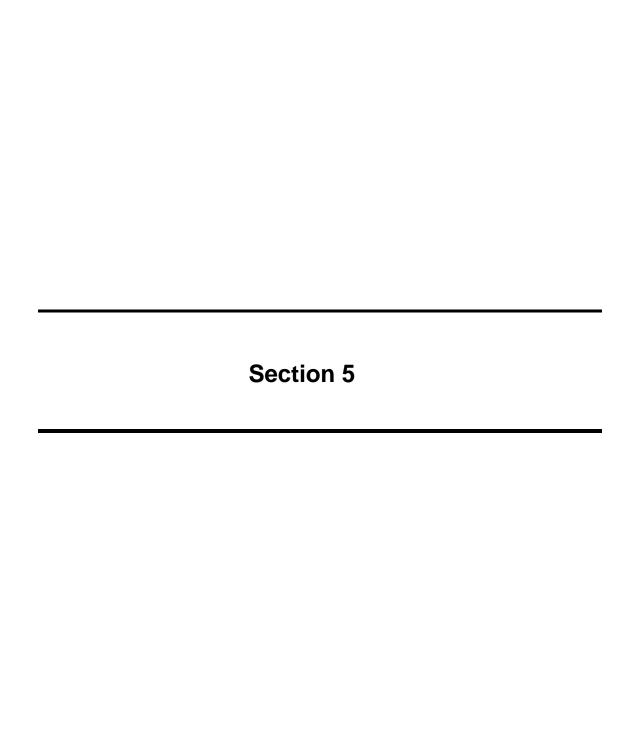


(b) Does the project have impacts that are individually limited, but cumulatively considerable? "Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.

Less than Significant Impact. CEQA Guidelines Section 15064(i) states that a Lead Agency shall consider whether the cumulative impact of a project is significant and whether the effects of the project are cumulatively considerable. The assessment of the significance of the cumulative effects of a project must, therefore, be conducted in connection with the effects of past projects, other current projects, and probable future projects. The proposed Harney Lane Specific Plan also serves as an implementing arm of the City's General Plan 2010. Chapter 5 of the General Plan designates Harney Lane as a four lane expressway from Lower Sacramento Road to State Route 99. To achieve this goal, the City proposes Harney Lane Specific Plan, which intends to refine the vision, goals, policies, and actions of the City's General Plan by establishing area-specific goals and policies to guide land use patterns in the Specific Plan Area (along Harney Lane). All the individual and cumulative impacts have been analyzed in the Lodi General Plan EIR 2009 (SCH#2009022075). As such, less than significant impacts are anticipated with full implementation of mitigation measures outlined in Lodi General Plan EIR 2009 (SCH#2009022075).

(c) Does the Project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

Less than significant impact. The proposed project would not cause any significant environmental impacts, either short term or long term. The project is designed to alleviate traffic congestion and provide standard road widths within an established community. The proposed project would not result in any adverse effects on human beings, either directly or indirectly.



Documents Referenced

- Alquist-Priolo Earthquake Fault Zoning Act (http: www.consrv.ca.gov/dmg/shezp/maps/mora4.htm).
- California Environmental Quality Act Guidelines, as amended.
- City of Lode General Plan 2010.
- City of Lodi General Plan Environmental Impact Report 2009 (SCH#2009022075)
- Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Map Panel Number 06077C0169F, Effective Date October 16, 2009.
- Guide For Assessing And Mitigating Air Quality Impacts., Prepared by San Joaquin Valley Air Pollution Control Distrct.
- State of California, Department of Conservation, Division of Land Resource Protection. Farmland Mapping and Monitoring Program. Accessed at www.consrv.ca.gov/dlrp
- State of California, Health and Human Safety Code, Section 7050.5.
- State of California, Public Resources Code, Section 5097.5.
- United States, Department of the Interior, Fish & Wildlife Service. National Wetlands Inventory. Wetlands Mapper, Accessed March 28, 2011. Available online at http://www.fws.gov/wetlands/data/Mapper.html
- United States, Department of the Interior, Fish & Wildlife Service. *The National Map* (created and maintained by U.S. Department of the Interior, Geological Survey).
- United States, Environmental Protection Agency, *EnviroMapper for Superfund*. Available online at http://www.epa.gov/enviro/sf/.



Please immediately confirm receipt this fax by calling 333-6702

CITY OF LODI
P. O. BOX 3006
LODI, CALIFORNIA 95241-1910

ADVERTISING INSTRUCTIONS

SUBJECT: PUBLIC HEARING TO CONSIDER RESOLUTION APPROVING

HARNEY LANE SPECIFIC PLAN REPORT AND CERTIFYING HARNEY LANE SPECIFIC PLAN NEGATIVE DECLARATION AS ADEQUATE ENVIRONMENTAL DOCUMENTATION FOR HARNEY

LANE SPECIFIC PLAN

PUBLISH DATE: SATURDAY, JUNE 18,2011

LEGAL AD

TEAR SHEETS WANTED: One (1) please

SEND AFFIDAVIT AND BILL TO: RANDI JOHL, CITY CLERK

LNS ACCT. **#0510052** City of Lodi **P.O.** Box 3006

Lodi, CA 95241-1910

DATED: THURSDAY, JUNE 16,2011

ORDERED BY: RANDI JOHL

CITY CLERK

JENNIFER M. ROBISON, CMC ASSISTANT CITY CLERK MARIA BECERRA ADMINISTRATIVE CLERK

Verify Appearance of this Legal in the Newspaper – Copy to File

| | The second second | | |
|--|--------------------------------|--|----------------|
| Envod to the Centinel of 260 1027 of | · Voldate | | ~~~~\ |
| Faxed to the Sentinel at 369-1084 at (time) on | (date | The second secon | pages) |
| | 10000000 | | |
| | Service Control of the Control | CONTRACTOR IN 1 - WHITE CONTRACTOR | |
| LNS Phoned to confirm receipt of all pages at (time) | | MB | JMR (initials) |
| LINO CONTROL SERVICE S | | AN ADDRESS OF THE PARTY OF THE | |
| The state of the s | 111111111111111 | | |



DECLARATION OF POSTING

PUBLIC HEARING TO CONSIDER RESOLUTION APPROVING HARNEY LANE SPECIFIC PLAN REPORT AND CERTIFYING HARNEY LANE SPECIFIC PLAN NEGATIVE DECLARATION AS ADEQUATE ENVIRONMENTAL DOCUMENTATION FOR HARNEY LANE SPECIFIC PLAN

On Thursday, June 16, 2011, in the City of Lodi, San Joaquin County, California, a Notice of Public Hearing to consider resolution approving Harney Lane Specific Plan Report and certifying Harney Lane Specific Plan Negative Declaration as adequate environmental documentation for Harney Lane Specific Plan (attached and marked as Exhibit A) was posted at the following locations:

Lodi Public Library Lodi City Clerk's Office Lodi City Hall Lobby Lodi Carnegie Forum

I declare under penalty of perjury that the foregoing **is** true and correct.

Executed on June 16, 2011, at Lodi, California.

ORDERED BY:

RANDI JOHL CITY CLERK

JENNIFER M. ROBISON, CMC ASSISTANT CITY CLERK

ADMINISTRATIVE CLERK



DECLARATION OF MAILING

PUBLIC HEARING TO CONSIDER RESOLUTION APPROVING HARNEY LANE SPECIFIC PLAN REPORT AND CERTIFYING HARNEY LANE SPECIFIC PLAN NEGATIVE DECLARATION AS ADEQUATE ENVIRONMENTAL DOCUMENTATION FOR HARNEY LANE SPECIFIC PLAN

On Thursday, June 16, 2011, in the City of Lodi, San Joaquin County, California, I deposited in the United States mail, envelopes with first-class postage prepaid thereon, containing Notice of Public Hearing to consider resolution approving Harney Lane Specific Plan Report and certifying Harney Lane Specific Plan Negative Declaration as adequate environmental documentation for Harney Lane Specific Plan, attached hereto marked Exhibit A. The mailing list for said matter is attached hereto, marked Exhibit B.

There is a regular daily communication by mail between the City of Lodi, California, and the places to which said envelopes were addressed.

I declare under penalty of perjury that the foregoing is true and correct.

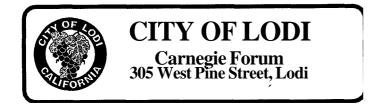
Executed on June 16, 2011, at Lodi, California.

ORDERED BY:

RANDI JOHL CITY CLERK, CITY OF LODI

JENNIFER M. ROBISON, CMC ASSISTANT CITY CLERK

ADMINISTRATIVE CLERK



Date: July 20,2011

Time: 7:00 p.m.



For information regarding this notice please contact:

Randi Johl,

City Clerk

Telephone: (209) 333-6702

NOTICE OF PUBLIC HEARING

NOTICE IS HEREBY GIVEN that on **Wednesday, July 20, 2011,** at the hour of 7:00 p.m., or as soon thereafter as the matter may be heard, the City Council will conduct a public hearing at the Carnegie Forum, 305 West Pine Street, Lodi, to consider the following matter:

a) Resolution approving Harney Lane Specific Plan Report and certifying the Harney Lane Specific Plan Negative Declaration as adequate environmental documentation for the Harney Lane Specific Plan.

Information regarding this item may be obtained in the Public Works Department, 221 West Pine Street, Lodi, (209) 333-6706. All interested persons are invited to present their views and comments on this matter. Written statements may be filed with the City Clerk, City Hall, 221 West Pine Street, 2nd Floor, Lodi, 95240, at any time prior *to* the hearing scheduled herein, and oral statements may be made at said hearing.

If you challenge the subject matter in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice or in written correspondence delivered to the City Clerk, 221 West Pine Street, at or prior to the close of the public hearing.

By Order of the Lodi City Council:

City Clerk

Dated: June 15,2011

Approved as to form:

D. Stephen Schwabauer City Attorney



Mailing list for Harney Lane Specific Plan Report (July 20, 2011)

| Organization | Last Name | First Name | Mailing Address | City | State | Zip |
|--|-----------------|------------------------|------------------------|--------------|-------|-------------------------|
| | Abarca | Marvin and Maria | 2543 Ivory Lane | Lodi | CA | 95242-4811 |
| I and the state of | Aberle | Steven | 712 McCoy Court, #66 | Lod. | CA | 95240-7167 |
| The second secon | Aguayo | Salvador | 700 McCoy Court, #51 | Lodi | CA | 95240-7162 |
| Section 1997 Annual Control of the C | Ali | Ramzan | ,249 Dunsmuir Drive | Lodi | CA | 95240 |
| AND THE RESIDENCE OF THE PROPERTY OF THE PROPE | Allen | Michaeland Kathryn | 14315El Macero Drive | Davis | CA | 95618-4307 |
| | Alvarado | Jose _ | 1315 Harney Lane | Łodi | ·CA | 95242-4500 |
| · | 1 - • | Joe and Sandy | ,533 E. Harney Lane | Lodi | CA | 95242-9585 |
| are . | Amador | (Antonioand Évelia | 2062 Henderson Way | Lodi | CA | 95242-4814 |
| | ' <u>Amaral</u> | Robert and Andrea | 1946 Victoria Drive | <u>Lo</u> di | CA | 95242-4768 |
| managama Pag | Amick | Philip and D. | ,2526 Banyan Drive | Lodi | CA | 95240-7 1 04 |
| | Anderson | Brian and Venus | 2545 Pinkerton Way | Lodi | | 95242-4803 |
| _ | | Charles | 19836 Honey Bear Lane | Stockton | 'CA | 95209-1449 |
| | iAnthony | Bruce and Marie | | | ,CA | 95220-9207 |
| | \Archer | Carrie | 2070 Bishop Way | Lodi | CA | 95242-4813 |
| | Armstrong | Gary | 3020 Cumbria Court | Lodi | CA | 195242 |
| | Arquilada | Lydia | 1015 Harney Lane | 'Lodi | CA | 95240-7006 |
| | Aslam | Mohammad and Kulsoom | 2252 Katzakian Way | Lodi | CA | 95242-4799 |
| | Athanasiou | George and Kari | 18445 Rodeo Drive | Lodi | CA | 95240-9212 |
| For the control of th | Badyal | Jasbir Singh | 184 E. Harney Lane | Lodi | CA | 95242-9503 |
| and a many | Baker | A. Fred | P.O. Box 1510 | Lodi | CA | 95241-1510 |
| | Bakken | Troy and Loris | 1001 Harney Lane | Lodi | CA | 95240-7006 |
| | Barnes | Sandra | 278 Mulberry Circle | Lodi | CA | 95240-7113 |
| | Barnett | Christopher and Tricia | 2310 Olson Drive | Lodi | CA | 95242-4798 |
| | Baroni | Greg and Lisa | 2227 Olson Drive | Lodi | CA | 95242-4797 |
| | Batch | Delmar | 11174 N. Davis Road | Lodi | CA | 95242 |
| _ | Bauer | Randall | 2439 Rockingham Circle | Lodi | CA | 95242-4556 |
| | Becerra | Valentin and Elvira | 102 E. Harney Lane | Lodi | CA | 95242-9503 |
| | Beckman | Marcia | P.O. Box 1537 | Lodi | CA | 95241-1537 |
| | Bell | Lyndle | 814 E. Harney Lane | Lodi | CA | 95242-9534 |
| | Benov | Jarret and Kelly | 1727 Victoria Drive | Lodi | CA | 95242-4792 |
| | Bertini | Tom | 1452 Wildwood Drive | Lodi | CA | 95242-4779 |
| | Bice | Walter | 2433 Rockingham Circle | Lodi | CA | 95242-4556 |
| | Bond | Edward | P.O. Box 1747 | Lodi | CA | 95241-1747 |
| | Bond | Jennifer | 1215 W. Harney Lane | Lodi | CA | 95242 |
| are the day of the appropriate o | Braden | Ronald and Anne | 36 E. Harney Lane | Lodi | CA | 95242-9503 |
| | Bradley | Eric and Anna | 1530 Wildwood Drive | Lodi | CA | 95242-4780 |

| | } | 11 | local L | | | 1 |
|--|----------------------|---|--|--|-----------|-------------|
|) NO W character was a second | Bradley | Lucas and Amanda Cory and Lana | 2561 Ivory Lane 1542 Wildwood Drive | Lodi 'Lodi | CA. CA | 95242-4811 |
| a control do cho proprio contrato contrato control de la control de control d | Brazil | Andrew and Christine | | 1 | | |
| | - | | 2234 Katzakian Way | Ledi | CA | 95242-4700 |
| PRESIDENT STATES | Brogle | Kunt and Rita | , | Lodi | CA | 95242-4500 |
| The second states and the second states are second states and second states are second states and second states are seco | Brooks | Phyllis | | Lodi | ļ | i95242-4787 |
| And the state of t | Brunmeier | E AND ADDRESS OF THE PROPERTY | 1456 Springhaven Way | ************************************** | CA | 95242-4777 |
| *************************************** | Burke | Leo and Lanise | 2491 Maggio Circle | Lodi | CA | 95240-8811 |
| | Busarow | Betty | 1050 Bradford Circle | Lodi | CA | 95240-7002 |
| | Button | Robert and Ellen | 2203 Olson Drive | Lodi | CA | 95242-4797 |
| | Camacho | Eduardo and Leticia | P.O. Box 690340 | Stockton | CA | 95269-0340 |
| | Caraang | Amurefina and Marcelino | 2245 Olson Drive | Lodi | CA | 95242-4797 |
| | Carouba | Michael | 103 Applewood | Lodi | CA | 95242 |
| | Cassel | Jennifer | 1524 Wildwood Drive | Lodi | CA | 95242-4780 |
| | C astelanelli | Ricky Lee | בטאט w. Harney Lane | Lodi | CA | 95242 |
| | Castro | Esteban | 712 McCoy Court, #49 | Lodi | CA | 95240-7164 |
| 1 | Cervelli | Pauland Wendy | 1721 Victoria Drive | Lodi | CA | 95242-4792 |
| The second secon | Charkow | Samuel | P.O. Box 637 | Thornton | CA | 95686-0637 |
| . The constraint of the constr | Chesley | Stephen and Malay | 2072 Bishop Way | Lodi | CA | 95242-4813 |
| | Chien | Chia Te | 122 W. Harney Lane | Lodi | CA | 95242 |
| A STATE OF THE STA | Christopherson | Edward and Cassandra | 29 N. Allen Drive | Lodi | CA | 95242-2808 |
| 1 hr a | Clanton | John and Luana | 12210 Katzakian Way | Lodi | CA | 95242-4799 |
| | Claus | Nancy | 910 W. Harney Lane | Lodi | CA | 95242 |
| | Clyde | Scott | 1695 Castle Road | Sonoma | CA | 95476-8625 |
| A CONTROL OF THE PROPERTY OF T | Cockran | Callen and Stacy | 1536 Wildwood Drive | Lodi | CA | 95242-4780 |
| | Conti | Antonio and Cynthia | 13940 Simko Ranch Lane | Galt | CA | 95632-9023 |
| | Costa | Alvin and Hilda | 999 Pearwood Circle | Lodi | CA | 95242-2052 |
| The state of the s | Costamagna | John | P.O. Box 131 | Woodbridge | CA | 95258 |
| | Costanza | William and Teresa | 2557 Pinkerton Way | Lodi | CA | 95242-4803 |
| | Crabtree | P. A. | 1037 Vienna Drive | Lodi | CA | 95242-9695 |
| | Cruz | Atabulfo | 2520 Banyan Drive | Lodi | CA | 95240-7104 |
| | Cunha | Albert and J. | 1327 Harney Lane | Lodi | CA | 95242-4500 |
| | Dabaco | David and Roxanna | 1924 Victoria Drive | Lodi | CA | 95242-4768 |
| | Dais | John | 585 Springer Lane | Lodi | CA | 95242 |
| | Dais | Sam and Elsie | 585 E. Springer Lane | Lodi | CA | 95242-9224 |
| | Damaso | Bryan | 2549 Ivory Lane | Lodi | CA | 95242-4811 |
| | Davis | Mike | 1818 Victoria Drive | Lodi | CA | 95242-4769 |
| | Della | Maggiora | 2719 Stockton Street | Lodi | CA | 95240-8817 |
| I have a second to the second | Della | INIGERIOIG | Z/13 Stockton Street | Loui | | 1-0-10 00-1 |

| | Delongpre | John and Donna | 1548 Wildwood Drive | Lodi | CA | 95242-4780 |
|--|--|--|-------------------------|--------------------------------------|-----------|--|
| 2 And the second street of the | Deluchi residence | and any or the supplier of the | P.O. Box 682 | Lockeford | CA | 95237-0682 |
| 3 | Demetras | Norma | 1218 Bradford Circle | Lodi | CA | 95240-7036 |
| A C CAC SPACE IN SECTION 10 TO | Denniston | William and Betty | 2251 Olson Drive | Lodi | CA | 95242-4797 |
| The state of the s | Dhaliwal | Harinder and K. | 20800 N. Ray Road | Lodi | CA | 95242 |
| | Dibble | Marcella | 1020 Bradford Circle | Lodi | CA | 95240-7002 |
| Secretary of the Control of the Cont | Dietrich | Norene | 463 E. Harney Lane | Lodi | CA | 95242-9581 |
| ACCUPATION OF THE PROPERTY OF | Durston | David and Shelba | 12049 N. Angier Road | Lodi | CA | 95240-9479 |
| | Dyas | Jason | 569 Springer Lane | Lodi | CA | 95242-9224 |
| , | | Clinton and Marjorie | 2445 Rockingham Circle | Lodi | CA | 95242-4556 |
| ! | Ebert | Almeda | 12535 Crown Place | lLodi | CA | 95242-4787 |
| - | Ehlers | | 530 S. Mills Avenue | - make amountains in passes who side | | |
| a main sanns or me ara and membrahaming | Everitt | **rore of tribber 1 * namequation of an extraordiscolorist and discolorist and | 1320 E. Harney Lane | TO AND AND AND AND WATERWAY. | | er fan er trad Gregorian men nem am ar are en tra |
| Military & Administration of parameters result representation of the control of t | after Principles of the Control of | | 1002 E. Harney Lane | Lodi | CA | 95242-9534 |
| | | Nick and Jesslyn | 1649 E. Harney Lane | Lodi | CA | 95242 |
| Taken and the second se | | Robert and Clare | 1839 Scarborough Drive | Lodi | CA | 95240-6121 |
| | Fincher | | P.O. Box 352 | Clements | CA | 95227-0352 |
| mandania communicacia di | Fink | Carl | 540 S. Mills Avenue | Lodi | CA | 95242 |
| A Ministra | Freeman | Jonathan and Brooke | 2086 Henderson Way | Lodi | CA | 95242-4814 |
| * Neby: | Freitas | William and Esperanza | 1021 Harney Lane | Lodi | CA | 95240-7006 |
| THE STATE OF AN EXCHANGE STATE OF A SECURITION | Galamay | Jimmy and Cecilia | 428 Cedar Court | Lodi | CA | 95240-7141 |
| Al in Agency way and high control statements in the many | Galatsatos | George | 2316 Olson Drive | Lodi | CA | 95242-4798 |
| May Scotter Autoric community is required. MA | Garcia | Jon and Linda | 2068 Henderson Way | Lodi | CA | 95242-4814 |
| and a supplement of the supple | Garcia | Tomas and Martha | 1812 Victoria Drive | Lodi | CA | 95242-4769 |
| | Garrison | Flora | 227 Mulberry Circle | Lodi | CA | 95240-7108 |
| serreparador a compani - Mi Pilidollo embologo de contrata de Colonida (Colonida Colonida Col | Gates | The second secon | 540 E. Harney Lane | | CA | 95242-9585 |
| POWER A PROPERTY AND ADMINISTRATION ADMINISTRATION ADMINISTRATION AND ADMINISTRATION | Gaydon | William and June | | | <u>::</u> | 05242 4500 |
| | | Bernard and Helen | | 1 | CA | 95240-7114 |
| | | Clifford and Millie | | 1 . | | |
| | | | | | | |
| | Getty | Leroy and Alcene | 2535 Banyan Drive | Lodi | CA | 95240-7170 |
| | Ghannam | Kalliope | 2220 Olson Drive | Lodi | CA | 95242-4797 |
| | Giorgi | Kenneth and Elizabeth | 3185 Rhododendron Drive | Florence | OR. | 97439-8990 |
| | Giuliani | Frank and Sheila | 1129 Harney Lane | Lodi | CA | 95240-7001 |
| | Goff | Dale and Tammy | 880 E. Harney Lane | Lodi | CA | 95242-9534 |
| | Gohl | Wayne | 409 S. Orange Avenue | Lodi | CA | 95240-3824 |
| | Gonzalez | Roberto and Elva | 1930 Victoria Drive | Lodi | CA | 95242-4768 |

| | Goonan | Janette and John | 500 W. Harney Lane | Lodí | CA | 95242 |
|--|---------------|-------------------------|-------------------------|----------|-----|------------|
| | Grady | James Jr. and Elizabeth | 1257 Rivergate Drive | Lodi | CA | 95240 |
| | Grelle | Jerry and Cathryn | 490 E. Harney Lane | Lodi | CA | 95242-9583 |
| | Gresham | Mekeel | 2098 Henderson Way | Lodi | CA | 95242-4814 |
| | Grewal | Gurcharan | P.O. Box 665 | Thornton | CA | 95686-0665 |
| | Griess | Florence and Ivan | 13250 N. Extension Road | Lodi | I . | 95242 |
| | Griffitts | William | 2635 S. Stockton Street | Lodi | CA | 95240-8804 |
| | Grunsky | Jackson and Marisa | 2546 Pinkerton Way | Lodi | CA | 95242-4803 |
| | Gunselman | Michael | 700 McCoy Court, #70 | Lodi | CA | 95240-7162 |
| | Gutierrez | Lidia | 233 Mulberry Circle | Lodi | CA | 95240-7108 |
| - And deline impediate of 1.1 to 37 sections do signature from the season of the section of the | Hagelie | Berdean and Caroline | 501 Springer Lane | Lodi | CA | 95242-9224 |
| A CONTRACTOR OF THE CONTRACTOR | Halbran | Elizabeth | 483 Springer Lane | Lodi | CA | 95242 |
| | Hall | Cliff | 401 Tioga Drive | Lodi | CA | 95242-2651 |
| | Hall | Frank | 2613 W. Harney Lane | Lodi | CA | 95242-9570 |
| The state of the s | Halldorson | Bruce and Michelle | 2228 Katzakian Way | Lodi | CA | 95242-4799 |
| | Halloran | Michael and Elizabeth | 483 Springer Lane | Lodi | CA | 95242-9224 |
| , , , , , , , , , , , , , , , , , , , | Halsey | Mark and Christina | 1821 Victoria Drive | Lodi | CA | 95242-4769 |
| | Hamner | Gene and Rebecca | 2451 Rockingham Circle | Lodi | CA | 95242-4556 |
| | Haro | David and Lucy | P.O. Box 501 | Lodi | CA | 95241-0501 |
| | Harr | Bobbie | 1037 Bradford Circle | Lodi | CA | 95240-7040 |
| | Hassan | Mohammad | P.O. Box 693153 | Stockton | CA | 95269-3153 |
| | Hauger | James and Tarah | 2094 Bishop Way | Lodi | CA | 95242-4813 |
| | Hauger | William and Sandra | 2547 Lynch Way | Lodi | CA | 95242-4800 |
| | Hausauer | Kenneth and Cynthia | 2508 Ham Lane | Lodi | CA | 95242-4549 |
| The state of the s | Hayn | Brian | 810 E. Harney Lane | Lodi | CA | 95242-9534 |
| and the second s | Heine | Debra | 275 Mulberry Circle | Lodi | CA | 95240-7153 |
| AND ADDRESS OF THE PROPERTY AND ADDRESS OF THE PROPERTY OF THE | Heinze | Dale and Sheri | 1528 Springhaven Way | Lodi | CA | 95242-4778 |
| , and the second | Hellwig | A. | P.O. Box 1872 | Stockton | CA | 95201-1872 |
| | Hendricksen | Steven and Becky | 263 Mulberry Circle | Lodi | CA | 95240-7153 |
| The second of th | Hensel | Doug and Suzanne | 2591 Greenfield Lane | Lodi | CA | 95242-4781 |
| and the state of t | Hernandez | Nicolas and Maria | 299 Mulberry Circle | Lodi | CA | 95240-7112 |
| | Hernandez | Steven and Michelle | 668 Harney Lane | Lodi | CA | 95242-9588 |
| And the state of t | Herrick | John and Lisa | 2215 Olson Drive | Lodi | CA | 95242-4797 |
| | Hickerson | Timothy and Jackee | 1031 Bradford Circle | Lodi | CA | 95240-7040 |
| | Hoagland | Marvin and Rizalina | 1806 Victoria Drive | Lodi | CA | 95242-4769 |
| | Holbert | Carol | 2463 Rockingham Circle | Lodi | CA | 95242-4556 |
| | Hollingsworth | Gene | 1701 S. Mills Avenue | Lodi | CA | 95242 |

| A STATE OF THE PROPERTY OF T | The state of the s | and the second s | ATT CLUB Daile | ipo | ۲ | 95242 |
|--|--|--|-------------------------|-------|------------|------------|
| | Hrovat | Bill | 1556 Iris Urive | - COG | 5 6 | 2777 |
| | Huckins | Ray | 527 Springer Lane | Lodi | 5 : | 95242-9224 |
| The state of the s | Hwuansavath | Sengsourisack and Vats | 2621 Stockton Street | Lodi | 5 | 95240-8804 |
| | itz | Harry and B. L. | 614 W. Harney Lane | Lodi | క | 95242 |
| The second secon | | Chong | 2431 Vintage Oaks Court | Lodi | 5 | 95242-9347 |
| A STATE OF THE PARTY OF THE PAR | amura | Shozo and Bess | 553 Springer Lane | Lodi | క | 95242-9224 |
| | | Brian and Melissa | 1955 Victoria Drive | Lodi | ర | 95242-4768 |
| | A CONTRACT OF THE CONTRACT OF | Chris | 2232 Olson Drive | Lodi | 8 | 95242-4797 |
| | S | David and Gail | 1445 Wildwood Drive | Lodi | გ | 95242-4779 |
| | - | Bruce | 2129 Sunwest Drive | Lodi | 5 | 95242 |
| | And the same of th | Baliit | 2506 Meadow Drive | Lodi | প্র | 95240-7136 |
| · · · · · · · · · · · · · · · · · · · | ounis | Timothy and Tate | 1438 Springhaven Way | Lodi | প্র | 95242-4777 |
| | | Donald | 422 Cedar Court | Lodi | প্র | 95240-7141 |
| | th. | Bryan and Andrea | 2576 Greenfield Lane | Lodi | 8 | 95242-4781 |
| | > | Michael and Myrna | 2541 Crown Place | Lodi | క | 95242-4787 |
| | 5 | Ronald and Wendy | 1533 Wildwood Drive | Lodi | প্র | 95242-4780 |
| And the state of t | | Dr. Chris | 816 W. Lodi Avenue | Lodi | 8 | 95240 |
| | | Lugman | 22 Mulberry Court | Lodi | క | 95240-7114 |
| | | Taj | 1112 Rivergate Drive | Lodi | ধ | 95242 |
| The state of the s | ski | William and Erna | 1458 Wildwood Drive | Lodi | క | 95242-4779 |
| The state of the s | And Annual Co. Co. Comp. (1977) Annual Co. | Leslie (Wayne) | 2031 E. Harney Lane | Lodi | ర | 95242 |
| Company Company | The state of the s | Wavne | 2031 W. Harney Lane | Lodi | క | 95242 |
| | henman | Jason and Stephanie | 1728 Victoria Drive | Lodi | క | 95242-4792 |
| The second secon | Pr | Michael | 1936 Victoria Drive | Lodi | క | 95242-4768 |
| The state of the s | 4. | Jav and Tammy | 2579 Greenfield Lane | Lodi | გ | 95242-4781 |
| The second secon | kowski | Mark and Colleen | 2256 Olson Drive | Lodi | ర | 95242-4797 |
| | Lackvard | Donald and Susan | 1477 E. Harney Lane | Lodi | প্র | 95242-9592 |
| | And the second s | Bobbie | 73 Reynolds Way | Lodi | ধ | 95242-8333 |
| | | Philip | 2548 Colony Drive | Lodi | প্র | 95242-4774 |
| | Lara | Glenn and Luzette | 3237 Ivory Lane | Lodi | প্র | 95242 |
| | Lasick, Sr. | Frank | 790 W. Harney Lane | Lodi | প্র | 95242 |
| | Lawson | Joshua | 700 McCoy Court, #53 | Lodi | 5 | 95240-/162 |
| | Leach | Daniel and Rosa | 2547 Potomac Way | Lodi | ্ব | 95242-4770 |
| And the second s | Lee | Robert | 6504 Hogen Lane | Lodi | 8 | 95240 |
| Application of the state of the | Leeman | Ryan and Maria | 2322 Olson Drive | Lodi | 5 3 | 95242-4798 |
| | Leiker | Dennis and Anita | 2508 Banyan Drive | Lodi | 5 | 95240-7104 |
| The state of the s | \$ | Janet | 903 Elliot Place | Lodi | প্র | 95240 |
| | | The state of the s | | | | |

| | Leon | Rodrigo and Tiffany | 1815 Victoria Drive | Lodi | CA | 95242-4769 |
|--|-------------|----------------------|-----------------------------|------------|-----|--------------|
| | Liepart III | Lisbet and George | 907 W. Harney Lane | Lodi | CA | 95240 |
| The state of the s | Lind | Anthony and Sharon | 908 Interlaken Drive | Lodi | CA | 95242-9167 |
| | Lind | Edith | P.O. Box 410 | Lodi | CA | 95241-0410 |
| The state of the s | Love | Michael and Colleen | 800 Morning Dove Lane | Rocklin | CA | 95765-5348 |
| The second secon | Lucas | Aaron | 712 McCoy Court, #57 | Lodi | CA | 95240-7164 |
| | Madewell | Randy and Joellen | 1422 Wildwood Drive | Lodi | CA | 95242-4779 |
| | Magana | Mario and Martha | 293 Mulberry Circle | Lodi | CA | 9952440/7112 |
| | Manassero | Michael and Pat | 1490 E. Harney Lane | Lodi | ,CA | 95242-9589 |
| | Mangrum | James | 2524 Winchester Street, #15 | Lodi | CA | 95240-8114 |
| | Mardini | Tameem | 2064 Bishop Way | Lodi | CA | 95242-4813 |
| | Marquez | Octavio | 281 Mulberry Circle | Lodi | CA | 95240-7153 |
| | Mastel | Jeanine and Phyllis | 499 E. Harney Lane | Lodi | CA | 95242-9582 |
| | Matsumoto | Sam and Harriet | 1455 Springhaven Way | Lodi | CA | 95242 |
| | Mauch | William and Terry | 2521 Meadow Drive | Lodi | CA | 95240-7131 |
| | McCune | Daniel and Joyce | 220 Mulberry Circle | Lodi | CA | 95240-7154 |
| and the control of th | Mehrer | David and Deborah | 2240 Katzakian Way | Lodi | CA | 95242-4799 |
| | Merino | Marian | 1967 Victoria Drive | Lodi | CA | 95242-4768 |
| | Meyer | Joline | 2017 Cochran Road | Lodi | CA | 95242 |
| | Miller | Anne | 4589 Craig Lane | Vacaville | CA | 95688-9327 |
| The second secon | Miller | Michael and Sandra | 257 Mulberry Circle | Lodi | CA | 95240-7153 |
| 4 A C C C C C C C C C C C C C C C C C C | Mims | Judy | 2577 Poppy Drive | Lodi | CA | 95242-4776 |
| | Molles | James and Shelley | 4006 W. Woodbridge Road | Lodi | CA | 95242-9617 |
| The second of th | Moore | Stephanie | 2082 Bishop Way | Lodi | CA | 95242-4813 |
| | Moran | Carol | 2524 Winchester Street, #6 | Lodi | CA | 95240-8112 |
| | Mosqueda | Hilario and Rosa | 2239 Olson Drive | Lodi | CA | 95242-4797 |
| | Muhlbeier | Tim and Kathy | 1468 Springhaven Way | Lodi | CA | 95242-4777 |
| | Munson | Timothy and Christie | 2569 Poppy Drive | Lodi | CA | 95242-4776 |
| | Musgrove | Robert and Patricia | 1572 Misty Wood Drive | Roseville | CA | 95747-7900 |
| The state of the s | Nava | Jaime and Rosalva | 2525 Ham Lane | Lodi | CA | 95242-4550 |
| | Navarro | Margarita | 246 Mulberry Circle | Lodi | CA | 95240-7156 |
| | Nguyen | Thomas | 2552 Pinkerton Way | Lodi | CA | 95242-4803 |
| | Nicolaou | Steven | 2573 Greenfield Lane | Lodi | CA | 95242-4781 |
| | Oden | Michael | 1518 Wildwood Drive | Lodi | CA | 95242-4780 |
| | Ordaz | Jesus | 2074 Henderson Way | Lodi | CA | 95242-4814 |
| | Paiste | Manuel and Gregoria | 284 Mulberry Circle | Lodi | CA | 95240-7113 |
| | Paoletti | Jeanne | P.O. Box 1068 | Woodbridge | CA | 95258-1068 |

| 1 2002 1 2 200 | Pappous | Anastasios and C. | 5395 Entrada Olmos | San Jose | CA | 95123-1425 |
|--|-----------|----------------------|-----------------------------|------------|-------------|------------|
| · | Penner | Joseph and Jenny | 2080 Henderson Way | Lodi | CA | 95242-4814 |
| A CONTRACTOR OF THE PROPERTY O | Pennino | | 1450 Springhaven Way | Lodi | CA | 95242-4777 |
| | | Bryan Jeff and P. | 1026 Bradford Circle | Lodi | CA | 95242-4777 |
| BAS | Perlegos | | | | | 95240-7002 |
| | Persson | Phil and Colline | 1469 Wildwood Drive | Lodi | CA | |
| | Petterson | Joann | 612 McCoy Court, #62 | Lodi | CA | 95240 |
| . W | Phillips | Christopher | 2238 Olson Drive | Lodi | CA | 95242-4797 |
| Professional and the second and the | Pilcher | James and Susan | 239 Mulberry Circle | Lodi | CA | 95240-7108 |
| to Fit we split in the Copy tambe. The Property of we will differ appropriately Fit appropriately Fit for a property of the Copy tamber of the Cop | Pinnell | Robert and Letha | P.O. Box 155 | Victor | CA | 95253-0155 |
| · · · · · · · · · · · · · · · · · · · | Polenske | Ron and Jeanetta | 2520 S. Ham Lane | Lodi | CA | 95242-4549 |
| | Polk | !Julie | 2524 Winchester Street. #9 | Lodi | CA | 95240-8113 |
| The second secon | Pollock | Leland | 712 McCoy Court, #67 | Lodi | CA | 95240-7167 |
| , | Post | David | 700 McCoy Court, #64 | Lodi | CA | 95240-7162 |
| | Quezada | Javier and Maria | 502 E. Harney Lane | Lodi | CA | 95242-9584 |
| | Quizon | Ronald and Dana | 1244 Canyon Creek Drive | Newman | CA | 95360-2728 |
| | Ralstin | Stella | 2320 Sierra Highlands Drive | Reno | NV | 89523-2618 |
| | Reagan | Kevin | 2328 Olson Drive | Lodi | CA | 95242-4798 |
| } va | Reed | Tom and Cherie | 1908 Wyndham Way | Lodi | CA | 95242 |
| | Rehman | Mohammad | 700 McCoy Court, #12 | Lodi | CA | 95240-7162 |
| | Rendon | Anthony and Maria | 1007 Harney Lane | Lodi | CA | 95240-7006 |
| 1 | Renner | Robert | 245 Mulberry Circle | Lodi | CA | 95240-7108 |
| ļ | Reyes | Innias and Sandra | 446 Cedar Court | Lodi | CA | 95240-7143 |
| <u>~</u> | Reyno | Robert and Carolyn | P.O. Box 725 | Woodbridge | CA | 95258-0725 |
| 1 | Rice | Lesley | 13480 N. Extension Road | Lodi | CA | 95242-9249 |
| A About the second seco | Rico | Carlos and Elizabeth | 2056 Henderson Way | Lodi | CA | 95242-4814 |
| | Ridenour | Luella | 452 Cedar Court | Lodi | CA | 95240-7143 |
| A COMMENT OF THE PROPERTY OF T | Rieger | Earl and Naomi | 395 E. Harney Lane | Lodi | CA | 95242-9578 |
| | Rivera | Patricia | 2244 Olson Drive | Lodi | CA | 95242-4797 |
| a a substituting of a substituting and a substitution and a substituti | Robison | Scott | 2524 Winchester Street, #13 | Lodi | CA | 95240-8114 |
| | Rochert | Hans and Raina | 25 N. Wellington Way | Lodi | CA | 95242-3033 |
| | Rodriques | Kevin and Gail | 2538 Crown Place | Lodi | CA | 95242-4787 |
| i | Romero | Toribio and Ilma | 2524 Winchester Street, #14 | Lodi | CA | 95240-8114 |
| | Rostomily | Donald and Sandra | 1117 Harney Lane | Lodi | CA | 95240-7001 |
| ····· | Rubiolo | Michael | 2524 Winchester Street, #11 | Lodi | CA | 95240-8113 |
| F - Allegan, and the second se | | Mark and Elvira | 254 Mulberry Circle | Lodi | CA | 95240-7156 |
| ļ | Ruggiero | Outa | ,1918 Victoria Drive | Lodi | CA | 95242-4768 |
| 1 | Saetern | | 366 Camino Del Postigo | Escondido | CA | 92029-7438 |
| L = | Salomon | Greg and Laura | 1300 Carrillo Del Postigo | Lacondido | 124 | 32023 7430 |

| | Samra | Akhtar | 892 Almarida Drive | Campbell | CA | 95008-0125 |
|--|-------------------|-----------------------|---------------------------------|-----------|----|--------------------------|
| | Sanchez | Job and Elena | 909 E. Harney Lane | Lodi | CA | 95242-9591 |
| | Sanchez | Luis and Angela | 1634 E. Harney Lane | Lodi | CA | 95242-9588 |
| Learning as the excellent florent has assessed a control florent assessment fill the delication as a second assessment for the control as a second as | Sandoval | Jimmie | 1451Springer Lane | | | 95242-9224 |
| | Sandoval | Oscar and Ruth | Bradford Circle | | 1 | 95240-7002 |
| | Sandoval | Lorenzo and Margarita | 931 E. Harney Lane | 1 | CA | 95242-9591 |
| The state of the s | Santana | Fernando and Sarah | 266 Mulberry Circle | 1 | CA | 95240-7156 |
| - control of the cont | Sawyer | Jeffery | 434 Cedar Court | Lodi | CA | 95240-7143 |
| egy rangent rate MS to MS to go graphy (MS and the second | Scholl | James | 1100 E. Harney Lane | Lodi | CA | 95242-9534 |
| | Schumacher | Weldon and Bonnie | 1303 Rivergate Drive | Lodi | CA | 95240-0549 |
| Ma second 2 2 2 2 2 2 2 | Scott | Wayne | 692 E. Harney Lane | Lodi | CA | 95242 |
| | Seaton | Bobby and Mary | 10 Mulberry Court | Lodi | CA | 95240-7114 |
| | Segovia | Sergio and Maria | 2558 Hoff Lane | Lodi | CA | 95242-4815 |
| THE PERSON NAMED IN COLUMN TO A STATE OF THE PERSON NAMED IN COLUMN TO A STATE | Seibold | Joan | .1111Harney Lane | Lodi | CA | 95240-7001 |
| | Shah | Fiaz and Shahnaz | 1953 Victoria Drive | Lodi | CA | 95242-4768 |
| | Sharp | Kurt and Lisa | 2551 Pinkerton Way | Lodi | CA | 95242-4803 |
| - Annual Control of the Control of t | Shergill | Harbhajan | 1873 Jamestown Drive | Lodi | CA | 95242-4719 |
| | Sidhu | Nachhatar and Ruse | 5360 Gladstone Drive | Stockton | CA | 95219 |
| and the second s | Simmons | Ronald and Christine | 13444 Extension Road | Lodi | CA | 95242-9249 |
| and purpose of the state of the | Singh | Amarjit | 12553 Lynch Way | Lodi | CA | 95242-4800 |
| | Singh | Harjit and Jasjit | 2250 Olson Drive | Lodi | CA | 95242-4797 |
| | Smith | Fred and Letha | P.O. Box 1441 | Lockeford | CA | 95237-1441 |
| | Sommer | Dwaine and Juanita | 1548 Springhaven Way | Lodi _ | CA | 95242-4778 |
| and the second of the second o | Sousa | Nicole | 2524 Winchester Street, #10 | Lodi | CA | 95240-8113 |
| The second secon | Stanley | Steven and Jan | 2221 Olson Drive | Lodi | CA | 95242-4797 |
| and the second s | Starkovich | Aaron and Lee | 2214 Olson Drive | Lodi | CA | 95242-4797 |
| | Steinkamp | Heather | 2524 Winchester Street, #3 | Lodi | CA | 95240-8112 |
| | Stewart | John | 2209 Olson Drive | Lodi | CA | 95242-4797 |
| year allow accounts which are it is a positive about a constraint for a constraint and the constraint and the constraint is an absolute print in the paragraphs. | Stice | Larry and Goreti | 893 E. Harney Lane | Lodi | CA | 95242-9590 |
| The state of the s | Swope | Matthew and Elaine | 1073 Bradford Circle | Lodi | CA | 95240-7040 |
| | Taddei | Alex and Rosalie | 2456 Rockingham Circle | Lodi | CA | 95242-4555 |
| OF THE COLUMN AND ADDRESS AND | Tamura | Joey | 788 W. Armstrong Road | Lodi | CA | 95242-9544 |
| | Tamura | Satoru and Ethel | 1220 E. Harney Lane | Lodi | CA | 95242-9534 |
| The second of the second section and the second sec | Tanabe | Joyce | 1040 W. Kettleman Lane, Ste. 1B | Lodi | CA | 95240-6056 95242-4814 |
| ·- <u>Cup-anner</u> | | Erim and Annette | 2092 Henderson Way | Lodí | CA | 95242-4814 |
| | Taser Thompson | Scott and Dorothy | 1044 Bradford Circle . | Lodi | CA | 95240-7002 |
| | 1110111193011 | Maria | 2514 Banyan Drive | Lodi | CA | 95240-7104 |

| | Tran | Long and Lien | Henderson Way | Lodi | CA | 95242-4814 |
|--|-------------|----------------------|-------------------------|--------------|----|------------|
| and the control of th | Troutman | Gary and Diane | 1722 Victoria Drive | Lodi | CA | 95242-4792 |
| , | Tsutsumi | Gary and Diane | 3725 E. Armstrong Road | Lodi | CA | 95240-9425 |
| CONTROL CONTRO | Tuitavuki | Robert and Michelle | 1722 Olson Drive | Lodi | CA | 95242-4798 |
| | Uriz | Faustino and Maria | 3725 arney Lane | Lodi | CA | 95242-9577 |
| The state of the s | Valente | Thomas and Loretta | 960 E. Harney Lane | Lodi | CA | 95242-9534 |
| | Van Alen | Theodore and Karen | 2555 Ivory Lane | Lodi | CA | 95242-4811 |
| The state of the s | Van Lear | Sonja | 2468 Rockingham Circle | Lodi | CA | 95242-4555 |
| 100 | Van Ruiten | Robert | P.O. Box 548 | Woodbridge | CA | 95258-0548 |
| A MARIE POR PROPERTY OF THE PR | Vannortwick | Aaron and Lisa | 1959 Victoria Drive | Lodi | CA | 95242-4768 |
| | Vargas | Margarita | 2532 Banyan Drive | Lodi | CA | 95240-7104 |
| | Vargem | Keith and Sandra | 1470 Wildwood Drive | Lodi | CA | 95242-4779 |
| The state of the s | Varner | Sean and Summer | 2601 S. Stockton Street | Lodi | CA | 95240-8804 |
| | Velasco | Arnulfo | 272 Mulberry Circle | Lodi | CA | 95240-7156 |
| The second secon | Vernon | John and Julie | 2585 Greenfield Lane | Lodi | CA | 95242-4781 |
| The second secon | Villanueva | Elissa | 1212 Bradford Circle | Lodi | CA | 95240-7036 |
| The state of the s | Villanueva | Gerardo and Brandee | 2540 Potomac Way | Lodi | CA | 95242-4770 |
| The state of the s | Vipond | Jeffrey and Anna | 2573 Canal Drive | Lodi | CA | 95242-4818 |
| The second secon | Viramontes | Abelardo | 425 Springer Lane | Lodi | CA | 95242-9224 |
| A STATE OF THE STA | Vocker | Robert and Carolyn | 1525 Wildwood Drive | Lodi | CA | 95242-4780 |
| A CONTROL OF A CON | Volkert | Nicholas | P.O. Box 2625 | Corvallis | OR | 97339-2901 |
| | Waldo | Roni | 700 McCoy Court, #61 | Lodi | CA | 95240-7162 |
| The second secon | Walsh | Kimberly | 2546 Hoff Lane | Lodi | CA | 95242-4815 |
| | Walz | James and Shirley | 110 Hemlock Drive | Lodi | CA | 95240-6737 |
| | Weasner | Wendy | 700 McCoy Court, #60 | Lodi | CA | 95240-7162 |
| 1 () () () () () () () () () (| Weisz | Christian | 2552 Hoff Lane | Lodi | CA | 95242-4815 |
| | Wells | Larry and Doris | 427 E. Harney Lane | Lodi | CA | 95242-9579 |
| | Wellwood | Lance and Stacey | 10295 Red Cedar Court | San Diego | CA | 92131 |
| | Wernette | Francis and B. | 1032 Bradford Circle | Lodi | CA | 95240-7002 |
| | Wichman | Bernell and S. | 1038 Bradford Circle | Lodi | CA | 95240-7002 |
| The second secon | Wild | Mark and Kathleen | 1807 Victoria Drive | Lodi | CA | 95242-4769 |
| | Williams | Charles and Jennifer | 8669 Bay Colony Drive | Indianapolis | In | 46234-2912 |
| | Williams | Chester and Robin | 1714 Timberlake Circle | Lodi | CA | 95242-4283 |
| | Williams | Janice | 1009 Bradford Circle | Lodi | CA | 95240-7040 |
| | Williams | Raylene | 260 Mulberry Circle | Lodi | CA | 95240-7156 |
| | Williams | Glenn and Barbara | 692 E. Harney Lane | Lodi | CA | 95242-9588 |
| | Winters | Traci | 2514 Ham Lane | Lodi | CA | 95242-4549 |

| a company of a market for the part of the | Wisenor | | 808 Tehama Drive | l od. | CA | 95242 |
|---|------------------|---------------------------|-----------------------------------|-------------|-------|------------|
| | Wombaugh | Dennis and Auguste | 1025 Bradford Circle | Lodi | CA | 95240-7040 |
| | Wong | Janice | 515 Swallow Lane | Lodi | CA | 95240-6388 |
| | Wong | Steve Wing Sing and Della | | Lodi | CA | 95240-0388 |
| [| Wright | Gregory and Karer | P.O. Box 1377 | Lockeford | CA | 95237-1377 |
| | Yarbrough | Michael and Mella | 1536 Springhaven Way | Lodi | CA | 95242-4778 |
| | Yost | | 817 Wightman Drive | Lodi | | 95242-3735 |
| | Young | Brian and Patricia | , 1056 Bradford Circle | Lodi | CA | 95240-7002 |
| | Young | Garvin and Stephanie | 1824 Victoria Drive | Lodi | CA | 95242-4769 |
| | Zarate | Isaac and Donna | 2036 Bishop Street | Stockton | CA | 95205-3442 |
| African American Chamber of Commerce | Amous | Paulette | 6333 Pacific Avenue, #537 | Stockton | CA | 95207 |
| American Medical Response | Dispatch | Tualcuc | 4701 Stoddard Road | Modesto | CA | 95356 |
| | Barkett | Fdward | 2800 W. March Lane, Ste. 250 | Stockton | CA | 95219-8218 |
| Atlas Properties, Inc. Baumbach & Piazza, Inc. | Elson | Josh | 323 W. Elm Street | | CA | 95240 |
| Baumbach & Piazza, Inc. | Pechin | Steven | 323 W. Elm Street | Lodi | CA CA | 95240 |
| Bennett Development | Bennett | Dennis | P.O. Box 1597 | Lodí | CA | 95 |
| Bennett Development | Moore | Steve | P.O. Box 1597 | Lodi | CA | 952412 |
| BIA of the Delta | Beckman | John | 315 N. San Joaquin Street, Ste. 2 | Stockton | CA | 95202 |
| Builders Exchange | Self | Mike | 7500 West Lane | Stockton | CA | 95210 |
| Business Council, Inc. | Addington | Ron | 2800 W. March Lane, #473 | Stockton | CA | 95219 |
| CA State Assembly | Huber | Hon. Alyson | 218 W. Pine Street | Lodi | CA | 95240 |
| CA State Assembly CA State Senate | Berryhill | Hon. Tom | 1308 W. Main Street, Ste. C | Ripon | CA | 95366 |
| CA Valley Miwok Tribe | Burley | Silvia | 1163 E. March Lane, Ste. D | Stockton | CA | 95210 |
| California Equity Mgmt Grp Inc | Property Manager | | P.O. Box 1747 | Modesto | CA | 95353-1747 |
| Central Valley Assoc. of Realtors | Coler | Cliff | 16980 S. Harlan Road | Lathrop | CA | 95330 |
| Cherokee Memorial Park | Irwin | Charles | 13823 N. Backman Road | Lodi | CA | 95240 |
| City of Lodi | Property Manager | | 221 W. Pine Street | Lodi | CA | 95240 |
| Cluff, LLC | Hanson | Richard | 908 W. Turner Road | Lodi | CA | 95242 |
| Conti & Associates, Inc. | | Antonio | P.O. Box 1396 | Woodbridge | CA | 95258 |
| CV Asian-American Chamber | 1200 | Dennis | 178.W. Adams | Stockton | CA | 95204 |
| Daisy Enterprises | Property Manager | | P.O. Box 1259 | Woodbridge | CA | 95258-1259 |
| Dillon & Murphy Engineering | Dillon | Cecil | P.O. Box 2180 | Lodi | CA | 95241-2180 |
| DRS Real Estate Appraisals | Sasaki | Darrell | 1806 W. Kettleman Lane, Ste. 1 | Lodi | CA | 95242 |
| F&L Costa Family LP | Costa | Felix _ | 13160 N. West Lane | Lodi | CA | 95242 |
| F&I Costa Family IP | Costa | Greg | 13160 N. West Lane — | Lodi | CA | 95240 |
| F&L Costa Family LP | Costa-Armstrong | Jane | 13160 N. West Lane | Lodi | CA | 95242 |
| CD Homes | Doucette | Tom | 10100 Trinity Parkway, Sta 420 | Stockton | CA | 95219 |

| FCB Homes | Jimison | Jim | 10100 Trinity Parkway, Ste. 420 | Stockton | CA | 95219 |
|----------------------------------|--|--|---------------------------------|------------|-----|------------|
| FF LP | Fink | Carl and Judith | 540 S. Mills Avenue | Lodi | CA | 95242-3428 |
| Frank Alberti Ranch LP | | The second secon | ,114026 N. Davis Road | Lodi | CA | 95242 |
| G and J Burns LLC | Meyer | Jolene | P.O. Box 1504 | Woodbridge | CA | 95258 |
| Geweke Properties | Farros | John | P.O. Box 1210 | Lodi | CA | 95241 |
| Glenbrough Homes | and the second s | Property Manager | P.O. Box 14 | Lodi | CA | 95240 |
| Greenlaw Grupe Operating Co Ptp | | Property Manager | P.O. Box 207007 | Stockton | CA | 95267-9507 |
| Harris & Associates | Roberts | Steve | 35 E. 10th Street, Ste. A | Tracy | CA | 95376 |
| Henderson School | Dosty, Principal | Allen | 13451 N Extension Road | Lodi | CA | 95242 |
| Herum Crabtree Attorneys | Aranda | Richie | 2291 W. March Lane | Stockton | CA | 95207 |
| Herum/Crabtree Attorneys | Herum | Steve | 2291 W. March Lane, Ste. B100 | Stockton | CA | 95207 |
| Hispanic Chamber of Commerce | Martinez | Mark | 306 E. Main Street, #303 | Stockton | CA | 95202 |
| Judith Buethe Communications | Buethe | Judith | P.O. Box 773 | Stockton | CA | 95201 |
| Judith Buethe Communications | Stanley | Jan | 445 W. Weber Avenue, Ste. 221 | Stockton | CA | 95203 |
| K and W Development LLC | | Property Manager | 816 W. Lodi Avenue | Lodi | CA | 95240-3302 |
| Kackys LLC | Meyers | Guy | 872 Westwind Drive | Lodi | CA | 95242 |
| Kackys LLC | Mikilas | Kathy | 872 Westwind Drive | Lodi | CA | 95242 |
| Katzakian, Williams, Sherman | Flemmer | Lowell | 777 S. Ham Lane, Ste. A | Lodi | CA | 95242 |
| KB Home North Bay, Inc. | | Property Manager | 2420 Del Paso Road | Sacramento | CA | 95834 |
| Kjeldsen, Sinnock & Neudeck | Sinnock | Steve | P.O. Box 844 | Stockton | CA | 95201-0844 |
| Lee & Associates | Davis | Tom | 241 Frank West Circle, Ste. 300 | Stockton | CA | 95242 |
| Lodi Chamber of Commerce | Patrick | Pat | 35 S. School Street | Lodi | CA | 95240 |
| Lodi City Clerk | John | Randi | 221 W. Pine Street | Lodi | CA | 95240 |
| Lodi City Council | Hansen | Hon. Larry | 221 W. Pine Street | Lodi | CA | 95240 |
| Lodi City Council | Katzakian | Hon. Phil | 221 W. Pine Street | Lodi | CA | 95240 |
| Lodi City Council | Mounce | Hon. JoAnne | 221 W. Pine Street - | Lodi | CA | 95240 |
| Lodi City Council | Nakanishi | Hon. Alan | 221 W. Pine Street | Lodi | CA | 95240 |
| Lodi City PIO | Hood | Jeff | 221 W. Pine Street | Lodi | CA | 95240 |
| Lodi Electrical Utility | Morrow | George | 1331 S. Ham Lane | Lodi | CA_ | 95242 |
| Lodi Fire Dept. | Pretz | Mike | 217 W. Pine Street | Lodi | CA | 95240 |
| Lodi Mayor | Johnson | Hon. Bob | 221 W. Pine Street | Lodi | CA | 95240 |
| Lodi Police Dept. | Main | David | 215 W. Elm Street, Ste. 240 | Lodi | CA | 95240 |
| Lodi Public Works | Sandelin | Wally | 221 W. Pine Street | Lodi | CA | 95242 |
| Lodi Unified School Dist | | Property Manager | 1305 E. Vine Street | Lodi | CA | 95240 |
| Ladi Unified School Dist. | Hand | Art | 1305 E. Vine Street | Lodi | CA | 95240 |
| Lodi-Woodbridge Grape Commission | Chandler | Mark | 2575 W. Turner Road | Lodi | CA | 95242 |
| Mark Thomas and Company, Inc. | Doty | Ken | 7300 Folsom Blvd., Ste. 203 | Sacramento | CA | 95826 |

| Mark Thomas and Company, Inc. | Himes | Rob | 7300 Folsom Blvd Ste. 203 | Sacramento | CA | 95826 |
|--------------------------------------|--------------|-------------------|-----------------------------------|-------------|------|-------------|
| McCoy North LLC | 4 100 \$ 100 | Property Manager | 310 James Way, #150 | | CA | 93449-2877 |
| MCR Engineering | Holtberg | Randy | 2000 O Street, Ste. 100 | | CA | 95814 |
| Miller Farms | Miller | Craig | 349 E. Harney Lane | Lodi | CA | 95240-6839 |
| Mohr Enterprises Ltd Ptp | Fry | Jerry | P.O. Box 97 | Hayward | CA | 94557-0097 |
| Montessori Villa School | Tarditi | Terry | 2525 Stockton Street | Lodi | CA | 134337 0037 |
| MV Transportation, Inc. | Kuykendall | Brenda | 24 S. Sacramento Street | Lodi | CA | |
| | Ellis | Mathew | 425 Merchant Street, Ste. 200 | Vacaville | | 95696 |
| Phillippi Engineering | | Ron | P.O. Box 1598 | Lodi | C A | 95090 |
| R Thomas Development, Inc. | Thomas | | | Stockton | CA | 95241-1398 |
| Riggs & Associates, Inc. | Ruiz | Sam | 4600 N. Pershing Avenue, Ste. D | | C/\ | 95207-6750 |
| S.J. Audubon Society | | | P.O. Box 7755 | Stockton | CA | |
| S.J. Council for the American Indian | | | P.O. Box 1552 | Manteca | CA | 95336 |
| S.J. Farm Bureau Federation | Valente | Joe | P.O. Box 8444 | Stockton | CA | 95208 |
| S.J. RTD | DeMartino | Donr | P.O. Box 201010 | Stockton | CA | 95201 |
| San Joaquin Partnership | Locke | Michael | 2800 W. March Lane, #473 | Stockton | CA | 95219 |
| Siegfried & Associates | Corrales | Lex | 3244 Brookside Road, Ste. 100 | Stockton | CA | 95219 |
| Sierra Club | | | P.O. Box 9258 | Stockton | CA | 95208 |
| SJ Co Public Works | Levers | Jeffrey | P.O. Box 1800 | Stockton | CA | 95201 |
| SJ Co. Board of Supervisors | Vogel | Hon. Ken | 44 N. San Joaquin Street, #627 | Stockton | CA | 95202 |
| SJ Co. CAO | Lopez | Manuel | 44 N. San Joaquin Street, #640 | Stockton | CA | 95202 |
| SJ Co. Community Development | Sullivan | Kerry | 425 N. El Dorado Street | Stockton | CA | 95202 |
| SJ Co. OES | Baldwin | Ron | 2101 E. Earhart Avenue, Ste. 300 | Stockton | CA | 95206 |
| SJ Co. PIO | McConnell | Karen | 44 N. San Joaquin Street | Stockton | CA | 95202 |
| SJ Co. Public Works | Gau | Tom | P.O. Box 1800 | Stockton | CA | 95201 |
| SJ Co. Public Works | Selling | Mike | P.O. Box 1800 | Stockton | CA | 95201 |
| SJ Co. Sheriff | Moore | Hon. Steve | 7000 Michael N. Canlis Blvd. | French Camp | CA | 95231 |
| SJ Co. Sheriff's Dept. | Desmarais | Lt. Tom | 7000 Michael Canlis Blvd. | French Camp | CA | 95231 |
| SJ Co. Sheriff's Dept. | Wright | Sgt. Todd | 7000 Michael Canlis Blvd. | French Camp | CA | 95231 |
| SJCOG | Chesley | Andy | 555 W. Weber Avenue | Stockton | | 95202 |
| SJCOG | Dial | Steve | 555 W. Weber Avenue | Stockton | CA | 95202 |
| SJRTD | Atherstone | Nathan | P.O. Box 201010 | Stockton | CA | 95201 |
| SJRTD | DeMartino | Donna | P.O. Box 201010 | Stockton | CA | 95201 |
| Sorour Partners | | Property Manager | 1342 Rivergate Drive | Lodi | CA | 95240 |
| Stokes Farms | | Property Manager | 7500 Kile Road | Lodi | CA _ | 95242 |
| Supervisor Vogel's Representative | Warmerdam | Denise | 44 N. San Joaquin Street | Stockton | CA | 95202 |
| Teichert Construction | Old | Troy | 265 Val Dervin Parkway | Stockton | CA | 95206 |
| | - Join | Property Manager | 3525 W. Benjamin Holt Drive | Stockton | CA | 95219-3403 |
| The Vineyard/Oaks at Lodi LLC | | Trioperty Manager | 15525 VV. Delijalilili Hole Drive | | L | |

| Tokay Development, Inc. | Kirst | Jeffrey | P.O. Box 1259 | Woodbridge | CA | 95258 |
|--------------------------------|--|------------------|------------------------------|------------|----|------------|
| U.S. Congress | McNerney | Hon. Jerry | 2222 Grand Canal Blvd., #7 | Stockton | CA | 95207 |
| Union Pacific Railroad Company | The second developed groups assumed a side of a depth AMARIAN Property of the Second Sec | Property Manager | 1416 Dodge Street, #830 | Omaha | NE | 68179-0001 |
| Valley Iron Works Inc | the largest of the entry to the state of the | Owner | 2205 Roaring Camp Drive | Gold River | CA | 95670-7619 |
| Valley Iron Works, Inc. | Coubal | Joe | 127 E. Harney Lane | Lodi | CA | 95240 |
| W L Investors | Doucette | Tom | 10100 Trinity Pkwy, Ste. 420 | Stockton | CA | 95219-7241 |
| Winchester Woods LLC | | Property Manager | P.O. Box 1070 | Woodbridge | CA | 95258-1070 |
| Wine & Roses | Munson | Russ | 2505 W. Turner Road | Lodi | CA | 95242 |
| Woodbridge Irrig Dist | Property Manager | Property Manager | 18777 Lower Sacramento Road | Woodbridge | CA | 95258-9122 |